

Figure 2.34a: Diagram for a typical shared amenity space

2.36 SUSTAINABLE DRAINAGE SYSTEMS (SUDS)

2.36.1 SuDS are an approach to managing surface water run-off which seeks to ensure that a proposed development is able to mimic natural drainage systems and retain water on the site, as opposed to traditional drainage approaches which involve piping water off site as quickly as possible. Where required, they should be integrated into the landscaping design at an early stage³⁹. SuDS Designers are advised to use the Council's Sustainable Drainage - Design and Evaluation Guide⁴⁰. The guide links the design of SuDS with the evaluation requirements of planning, in a sequence that mirrors the SuDS design process.

WHY USE SUDS?

2.36.2 SuDS offer significant advantages over conventional piped drainage systems in reducing flood risk by attenuating the rate and quantity of surface water runoff from a site, promoting groundwater recharge and biodiversity benefits, as well as improving water quality and amenity value.



Figure 2.36a: Green roof designed by Hayhurst & Co.



Figure 2.36b: Trench with planting. (Photo: James Hitchmough)



Figure 2.36c: Stepped roof with planters designed by Hayhurst & Co.



Figure 2.36d: Drainage pond. (Photo: 2b Landscape Consultancy)

³⁹ See Council's Validation Checklist at: <https://www.croydon.gov.uk/planningandregeneration/make-application/validation-checklist> for details of a range of matters which applicants should be aware of when submitting planning applications.

⁴⁰ See advice on the Council's role as the Lead Local Flood Authority at: <https://www.croydon.gov.uk/environment/flood-water/advice-to-planning-applicants>.

DESIGN OF SUDS

2.36.3 SuDS should be incorporated and integrated into the design of the landscape and buildings in suburban development to maximise landscaping and biodiversity opportunities. Appropriate options for SuDS in residential suburban developments include:

- **GREEN ROOFS:** A specially designed roof covering that absorbs water and attenuates flow to a drainage layer below.
- **FILLER TRENCHES AND DRAINS:** Shallow trenches filled with shingle and gravel to allow for temporary surface attenuation.
- **DRAINAGE SWALES AND PONDS:** Vegetated spaces that can be used to store excess water and may include an existing level of water and capacity provision. Drainage swales and ponds may also be included in a landscaping plan for development that is free from water except in case of flooding.
- **BIORETENTION SYSTEMS:** Contained, lowered landscaped areas or pre-fabricated units with soil and vegetation to reduce run-off.
- **PERMEABLE PAVING:** Permeable paving that allows water to filter down to the layer below and be discharged into a controlled drainage system.
- **RAINWATER AND/OR GREYWATER HARVESTING:** The easiest and most common form that can be provided is a household water butt, where it can be stored and used for gardening and other purposes at a later date. Greywater (water from showers, baths, basins etc.) needs to be treated if stored for any amount of time. This should be considered to achieve sustainability and building control objectives.



Figure 2.36e: Permeable paving.



Figure 2.36f: Water butts.

CASE STUDIES

2.37 REGINA ROAD

2.37.1 A good example of a **proposal for a back land** development on a site which is highly constrained, narrowing at one end. The development proposes a larger block where the site is wider, containing flats, and then a series of 1 and 2 storey houses. The change in scale reflects the proximity to existing neighbours. The use of high-quality contemporary materials differentiates the proposal from its surroundings, with architectural form that reinterprets traditional suburban building types to create unique homes that respond to issues of overlooking.

2.37.2 For more information, visit the planning public access register on the Council's website, using case number: 16/06023/FUL. The scheme was designed by Stitch Studio for Brick by Brick.



Figure 2.37a



Figure 2.37b

2.38 MULBERRY LANE

2.38.1 A good example of a ***sympathetic and faithful approach*** on a rear garden site. Well-chosen materials and considered detailing responds to the surrounding architecture. The development sits within the East India Conservation Area adjacent to other intensification examples, including the conversion and extension of existing properties into flats.



Figure 2.38a

2.39 ONSLOW GARDENS

2.39.1 A good example of an ***innovative and original approach*** in a rear garden development. The development provides 2 new family homes in the rear gardens of existing properties, with access being provided by the existing driveway of one of the host dwellings. The form and material approach is contemporary and seeks to enhance the local character by deliberately distinguishing itself from the existing street facing development.

2.39.2 For more information, visit the planning public access register on the Council's website, using case number: 16/00455/P



Figure 2.39a



Figure 2.39b



Figure 2.39c

2.40 RUSHDEN & RAVENSDALE

2.40.1 An example of 3 adjacent sites delivering a total of 28 homes in place of existing garages. Each site within the proposal presents a positive approach to suburban intensification. The proposal for a 7-storey block of flats on the **corner site** makes the most of its prominent location within the streetscene. Homes of 2 and 3 storeys located to the rear of existing dwellings are of a scale that respond to their context.

2.40.2 For more information, visit the planning public access register on the Council's website, using case number: 16/06374/FUL. The scheme was designed by HTA Design for Brick by Brick.



Figure 2.40a



Figure 2.40b



Figure 2.40c: Site plan.

2.41 MELVILLE AVENUE

2.41.1 The redevelopment of a single dwelling into 6 flats within a 3 storey + basement dwelling. This proposal exemplifies a good **contemporary reinterpretation approach** to character through the use of high-quality contemporary architectural design that makes a contextually considered response to the site and neighbourhood characteristics. The units are large and carefully planned, with generous window sizes. The landscaping and roof terraces make the most of the topography of the site, providing well considered communal amenity spaces.

2.41.2 For more information, visit the planning public access register on the Council's website, using case number: 17/00720/FUL. The scheme was designed by MATA Architects.



Figure 2.41a



Figure 2.41b

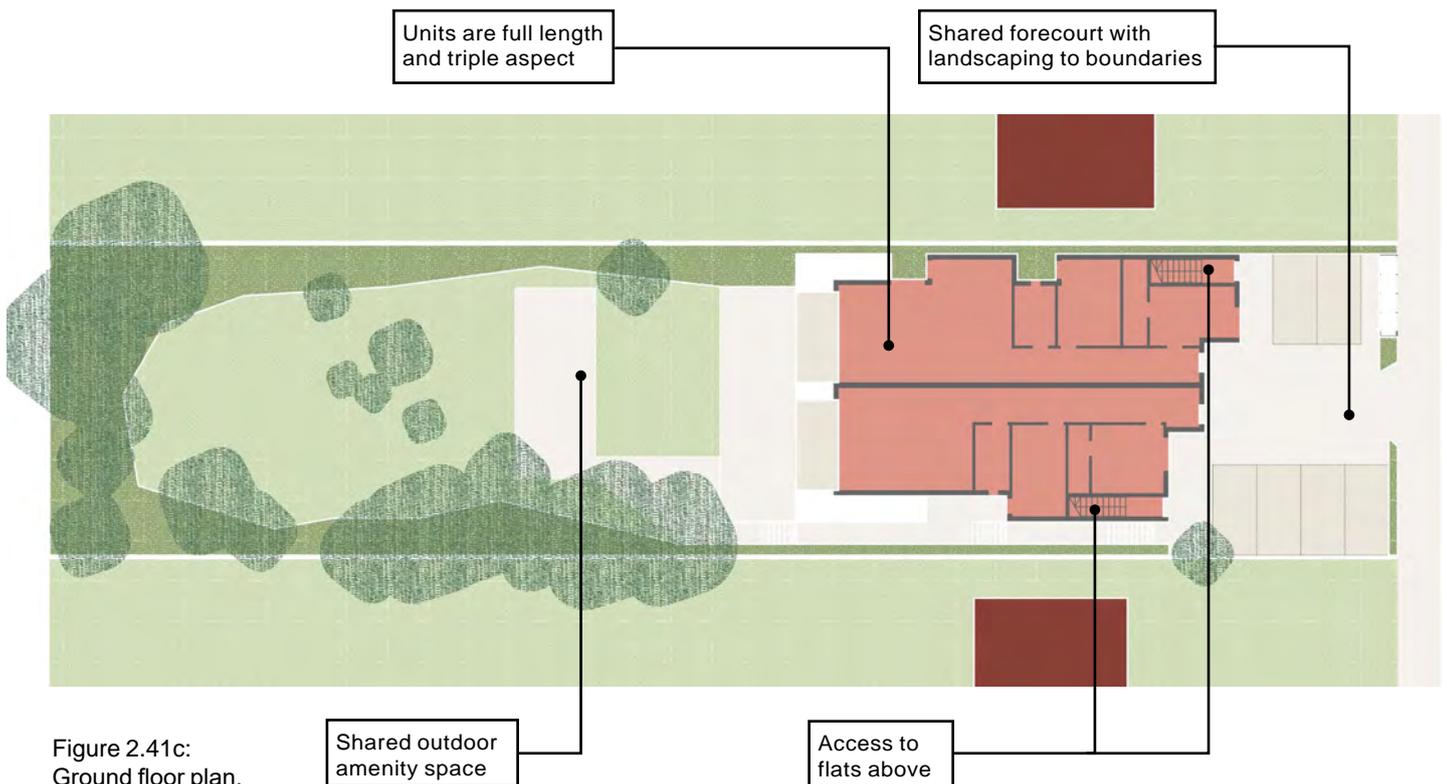


Figure 2.41c: Ground floor plan.

2.42 EAGLE HILL

2.42.1 The redevelopment of a constrained site formerly containing garages to provide 8 flats within a development that carefully steps down the site's steep topography. The homes are orientated around internal courtyards to bring light into deep plans and to prevent overlooking to neighbouring properties, whilst providing multiple outlooks. The building form is **innovative and original** but makes reference to the site's former use as garages.

2.42.2 For more information, visit the planning public access register on the Council's website, using case number: 16/06275/FUL. The scheme was designed by Coffey Architects for Brick by Brick.



Figure 2.42a



Figure 2.42b

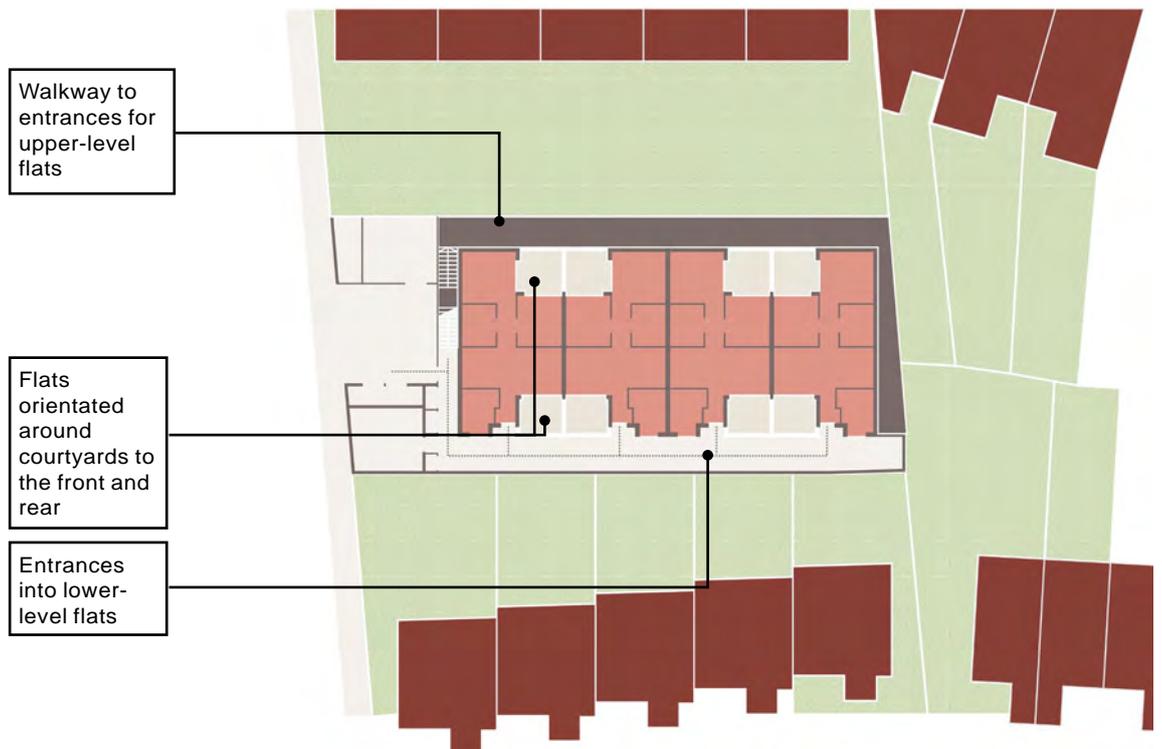


Figure 2.42c: Lower-level plan.

2.43 PAIR OF SEMI-DETACHED HOUSES

2.43.1 The redevelopment of 2 adjoining semi-detached properties, typically each being 3-bedroom dwellings. The proposal **optimises the site** to provide a high proportion of family-sized homes. There are 6 x 3-bedroom flats located in the street facing block, the third floor of which is partially contained within the roof-space. The rear garden development provides 2 x 2-bedroom houses that are inward facing so as not to prejudice development on neighbouring sites. The distribution of mass across the site reduces the impact of intensification on streetscape whilst providing a high percentage of family-sized units. The proposal that faces onto the street makes use of symmetry to respond to the context of the semi-detached street, with an enlarged building envelope to provide increased footprint to ensure the delivery of family-sized units. Parking is distributed across the site to minimise visual intrusion.

2.43.2 This is a designed scheme to highlight the possibility of such redevelopment.



Figure 2.43a



Figure 2.43b: Ground floor plan.

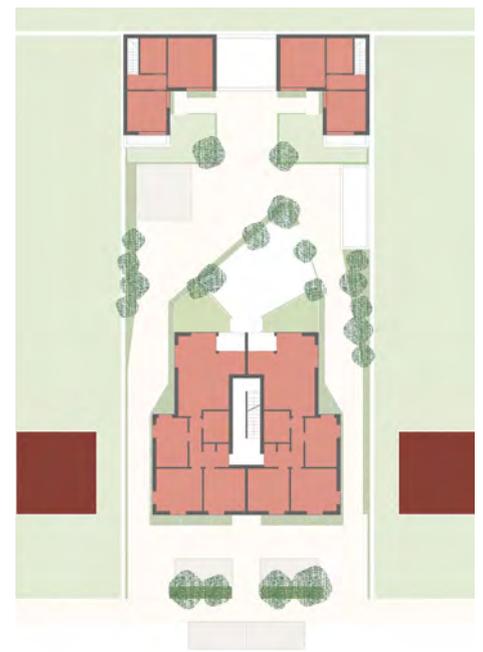


Figure 2.43c: First floor plan.

2.44 OVAL MEWS

2.44.1 Redevelopment of disused commercial/industrial buildings to provide 3 flats and 6 houses on an awkwardly shaped site with challenging overlooking issues. Where the proposal fronts the street it takes a **sympathetic and faithful approach**, matching materials and details to the neighbouring properties. To the rear, a mews style language is developed with a close-knit plan orientated around a shared access path. The layout ensures homes are dual aspect and have access to private outdoor amenity space.

2.44.2 For more information, visit the planning public access register on the Council's website, using case numbers: 5/01118/P. The scheme is a development by Chartwell Land & New Homes.



Figure 2.44a



Figure 2.44b

2.45 PURLEY DOWNS ROAD

2.45.1 A good example of the redevelopment of a single family house to provide 8 **family-sized homes**, each containing four-bedrooms. 2 houses face onto the road, with a further 6 houses set in the rear garden. The development is designed in a traditional style using high quality and robust materials that respond to the existing local architecture.

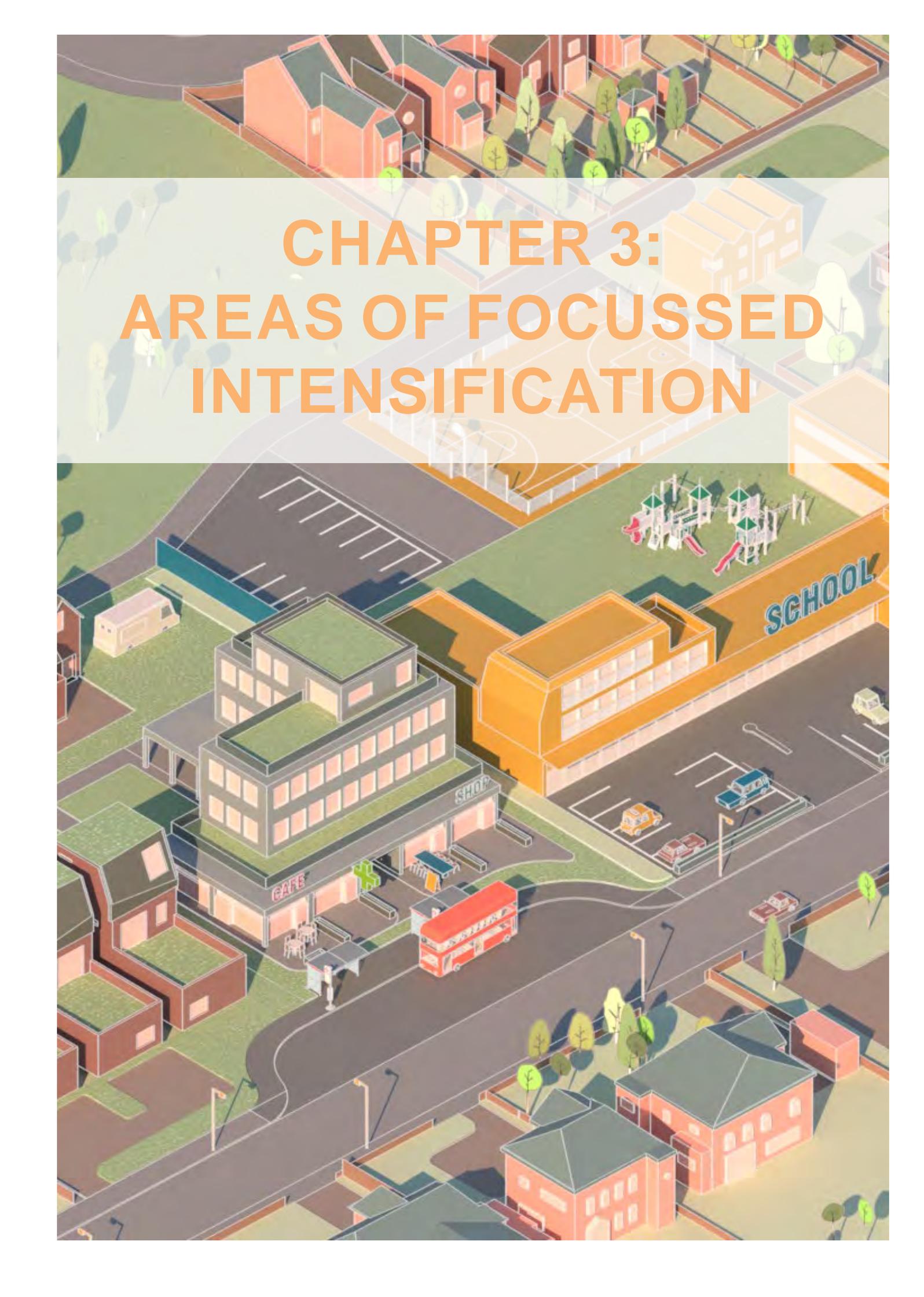
2.45.2 For more information, visit the planning public access register on the Council's website, using case number: 16/04186/FUL.



Figure 2.45a



Figure 2.45b

An aerial isometric illustration of a city street scene. The scene is divided into two horizontal sections. The top section shows a residential area with several houses and a school building. The bottom section shows a more developed urban area with a multi-story building featuring a 'CAFE' and 'SHOP' on the ground floor, a red double-decker bus, a playground, and a school building with 'SCHOOL' written on its side. The illustration uses a warm color palette of oranges, reds, and browns, with green for grass and trees. The text 'CHAPTER 3: AREAS OF FOCUSSED INTENSIFICATION' is overlaid in the center in a bold, orange, sans-serif font.

CHAPTER 3: AREAS OF FOCUSSED INTENSIFICATION

INTRODUCTION

3.1 DESIGNATION IN THE CROYDON LOCAL PLAN

3.1.1 To achieve the current housing target of the Croydon Local Plan, the Areas of Focussed Intensification were identified from evidence which indicated that they were areas with established infrastructure but relatively low density and the potential to accommodate a significant increase in residential development to meet the borough's housing target. The areas of focussed intensification are:

- The area around **Kenley** station;
- The area around **Forestdale** Neighbourhood Centre;
- **Brighton Road** (Sanderstead Road) Local Centre with its setting; and
- Settings of **Shirley** Local Centre and Shirley Road Neighbourhood Centre.

3.1.2 Policy DM10.11 of the Croydon Local Plan provides the policy against which development in areas of focussed intensification should be assessed against. It states that *'Developments in focussed intensification areas should contribute to an increase in density and a gradual change in character. They will be expected to enhance and sensitively respond to existing character by being of high quality and respectful of the existing place in which they would be placed'*.

3.1.3 Furthermore, the Croydon Local Plan sets out how Croydon will accommodate growth and improvement through different methods, one of which being focussed intensification associated with change of area's local character. Specifically, supporting text 6.103 states that

'focussed intensification aims to maximise the existing growth capacity through an increase in density of development and a gradual change in character to similar but higher density forms of development. Sites will be redeveloped with denser forms of development of a different character to that which exists in the local area currently as it would not be justified, when there is unmet housing need, to move towards a more consistent character that replicates surrounding low density development types'.

3.1.4 New development in Areas of Focussed Intensification may be significantly larger than existing and should;

- a. Be up to double the predominant height of buildings in the area;
- b. Take the form of character types "Medium-rise block with associated grounds", "large buildings with spacing", or "Large buildings with Continuous frontage line";
- c. Assume a suburban character with spaces between buildings.

3.1.5 Policy DM10.11 further states that intensification will be supported in and around District, Local and potential Neighbourhood Centres which have sufficient capacity for growth due to the high availability of community services. Further growth can be accommodated through more efficient use of existing infrastructure.

3.1.6 The Areas of Focussed Intensification have been designated due to their capacity to accommodate development. As such, these areas could relieve development pressure on more sensitive locations in the borough, including conservation areas or protected open spaces. It is expected that the evolution

of these places will result in a managed change of their character over a period of 10-20 years to meet the housing need.

3.1.7 As stated in Policy DM10.11 set out previously, intensification is expected to enhance and sensitively respond to existing character. Elements which contribute positively to the character of each Focussed Intensification Area – including public spaces, community facilities and infrastructure, Heritage Assets and Locally Designated Views – should be preserved and enhanced, and new development should be designed to respond positively towards them. Development should therefore consider Listed Buildings, Locally Listed Buildings, views and the relationship to the Metropolitan Green Belt land.

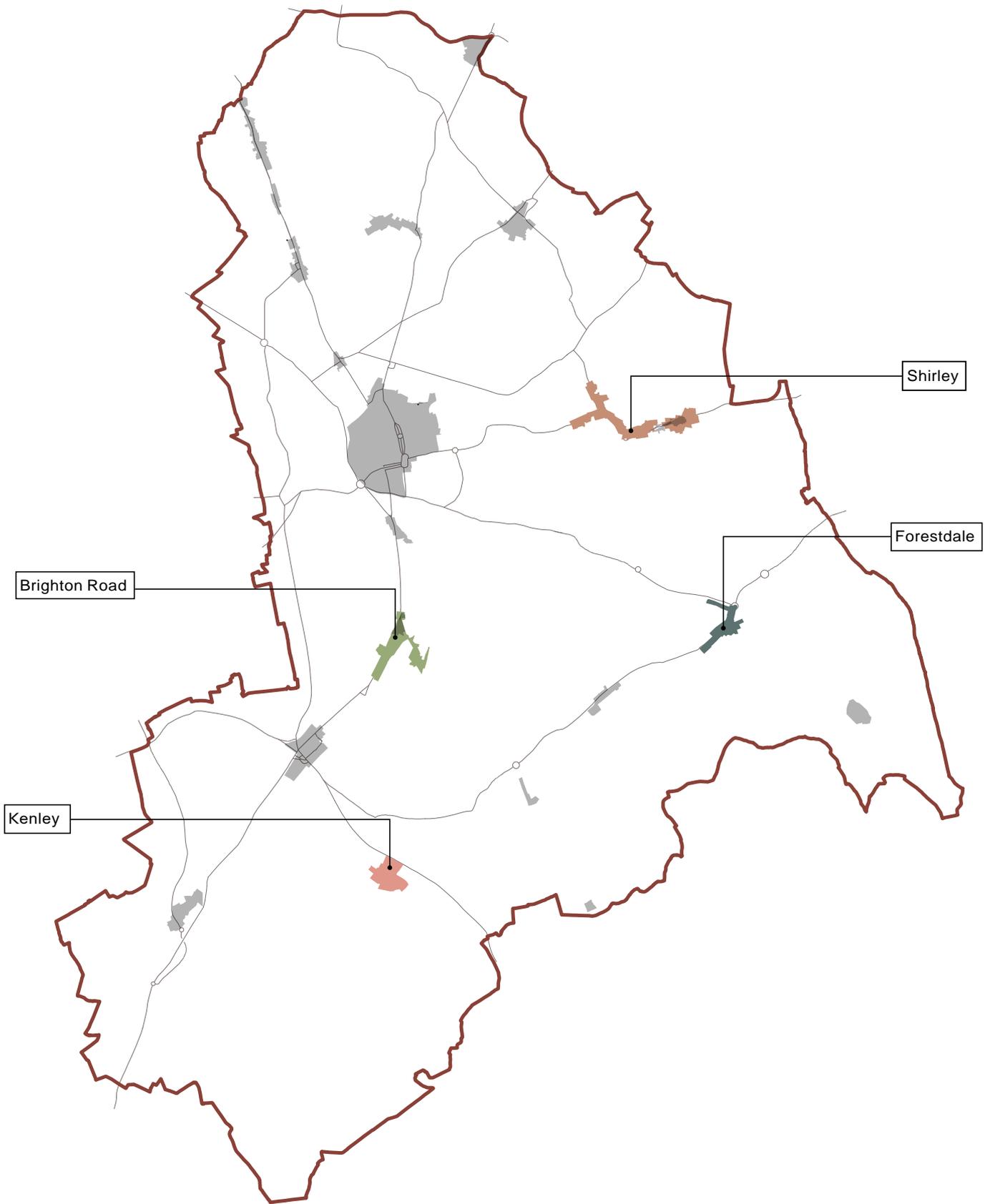


Figure 3.1a: Map of Croydon with Areas of Focussed Intensification highlighted and the Croydon Metropolitan Centre, District Centres and Local Centres shaded in grey which are all expected to accommodate intensification, along with Neighbourhood Centres where they have sufficient capacity for growth, in accordance with the Croydon Local Plan.

3.2 GENERAL GUIDANCE FOR AREAS OF FOCUSED INTENSIFICATION

3.2.1 Developments within the Areas of Focussed Intensification should primarily refer to the guidance within this chapter and, where relevant, refer to guidance within the previous chapter, 'Chapter 2: Suburban Residential Development'. The guidance on materials & external appearance, site layout & servicing, and landscaping & outdoor amenity space within Chapter 2 remain relevant. Policy DM10.11 of the Croydon Local Plan provides greater flexibility on massing and character for the Areas of Focussed Intensification than set out in Chapter 2, however it is still important that proposals develop an approach to character (refer to Section 2.7 & 2.8) that contributes to positive change and are aware of how the massing of a proposal will inform the future appearance of the area. All applications for residential extensions and alterations within the Areas of Focussed Intensification should refer to Chapter 4 for guidance.

3.2.2 As the number of residents increase in the areas, it provides the business case to improve and sustain services and facilities, providing tangible benefits that result from intensification. Whilst this guide is primarily a residential design guide and therefore cannot address all issues, the Council will seek opportunities to work with communities within the Intensification Areas to deliver this.

3.2.3 Beyond strengthening the provision of services, infrastructure and commercial offers in the areas, development should come forward in a manner that collectively promotes thriving, healthy and safe communities within the Intensification Areas. This includes contributing to biodiversity and recreational space through landscaping design both within private development sites and in the public realm in a manner that contributes to leafy suburban characteristics wherever possible.

3.2.4 The provision of sustainable transport facilities will be facilitated through transport improvement schemes such as the South Croydon Bus Review. Developments will be able to contribute to the development of sustainable transport options through promoting walking and cycling opportunities, for example in the design of access routes into a site and the provision of cycle storage as per the guidance in Chapter 2.

3.2.5 As demand on road infrastructure changes with reduced car ownership in line with national trends or where the need to address road safety issues emerges, the Council will seek to work with stakeholders and local communities to address these and wherever possible provide opportunities that will enhance the area.

3.2.6 The guidance in the following pages sets out a more detailed vision for each of the Focussed Intensification Areas and outlines indicatively the development potential within each area based on different building typologies. These building typologies are derived from the Croydon Typology Appraisal⁴¹. Applicants should refer to this for further information on the different typologies identified.

⁴¹ Available in the Urban Design, Local Character and Heritage section of the local plan evidence at: <https://www.croydon.gov.uk/planningandregeneration/framework>.

EVOLUTION OF STREET WITH A MIXED CHARACTER IN AN AREA OF FOCUSED INTENSIFICATION

2019



Figure 3.2a: **2019** - Medium-rise blocks of flats with associated garages sit opposite Victorian terraces. A mixture of buildings of different ages, underutilised garages and hardstanding dominates the street scene.

2036



Fig 3.2b: **2036** - Garages are redeveloped to provide new homes, whilst the existing flats and terraces are retained. Landscaping improves the street scene and shared bike storage is provided.

EVOLUTION OF A STREET WITH DETACHED & SEMI-DETACHED HOMES IN AN AREA OF FOCUSED INTENSIFICATION

2019



Figure 3.2c: **2019** - A mixture of detached and semi-detached homes bring variation to this street, but there is no dominant typology, while large gardens and landscaping shape the streetscene.

2036

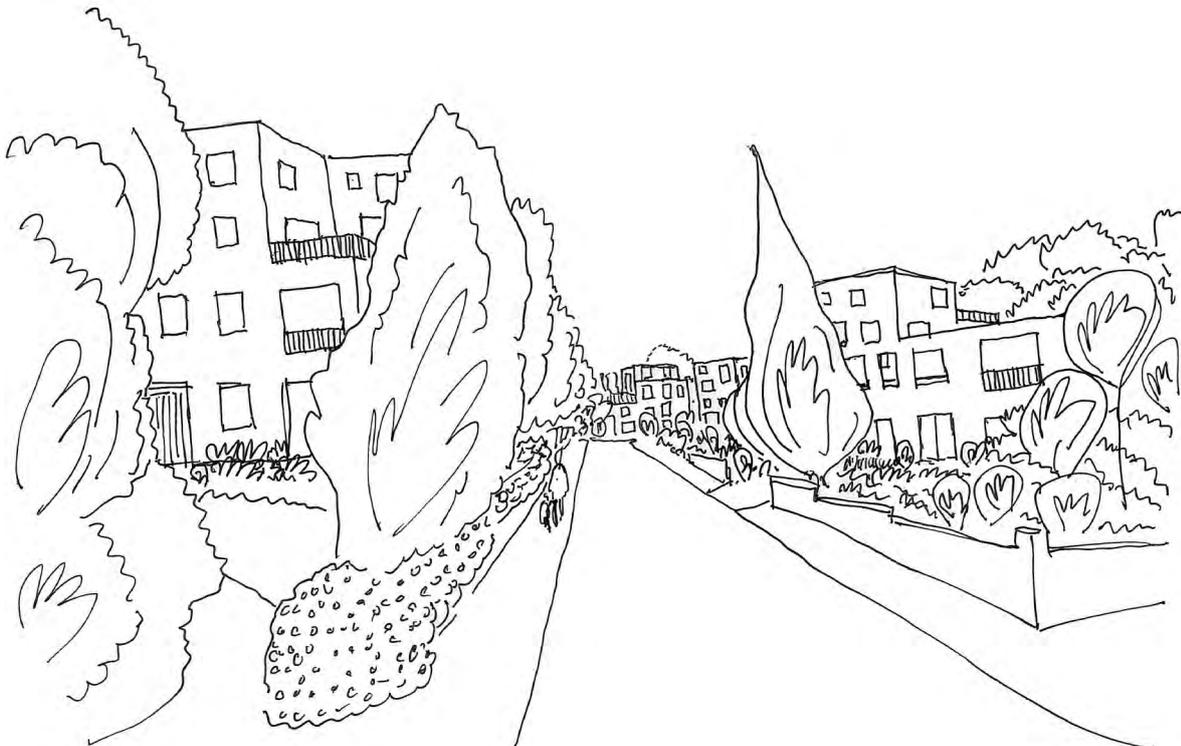


Figure 3.2d: **2036** - Redevelopment provides flats and townhouses set in generous gardens. The leafy character of the street is retained.

EVOLUTION OF AN ARTERIAL ROAD IN AN AREA OF FOCUSED INTENSIFICATION

2019



Figure 3.2e: **2019** - Housing occupies one side of the road, with a mixture of uses on the other. There is no predominant scale and the street scene is dominated by the road.

2036

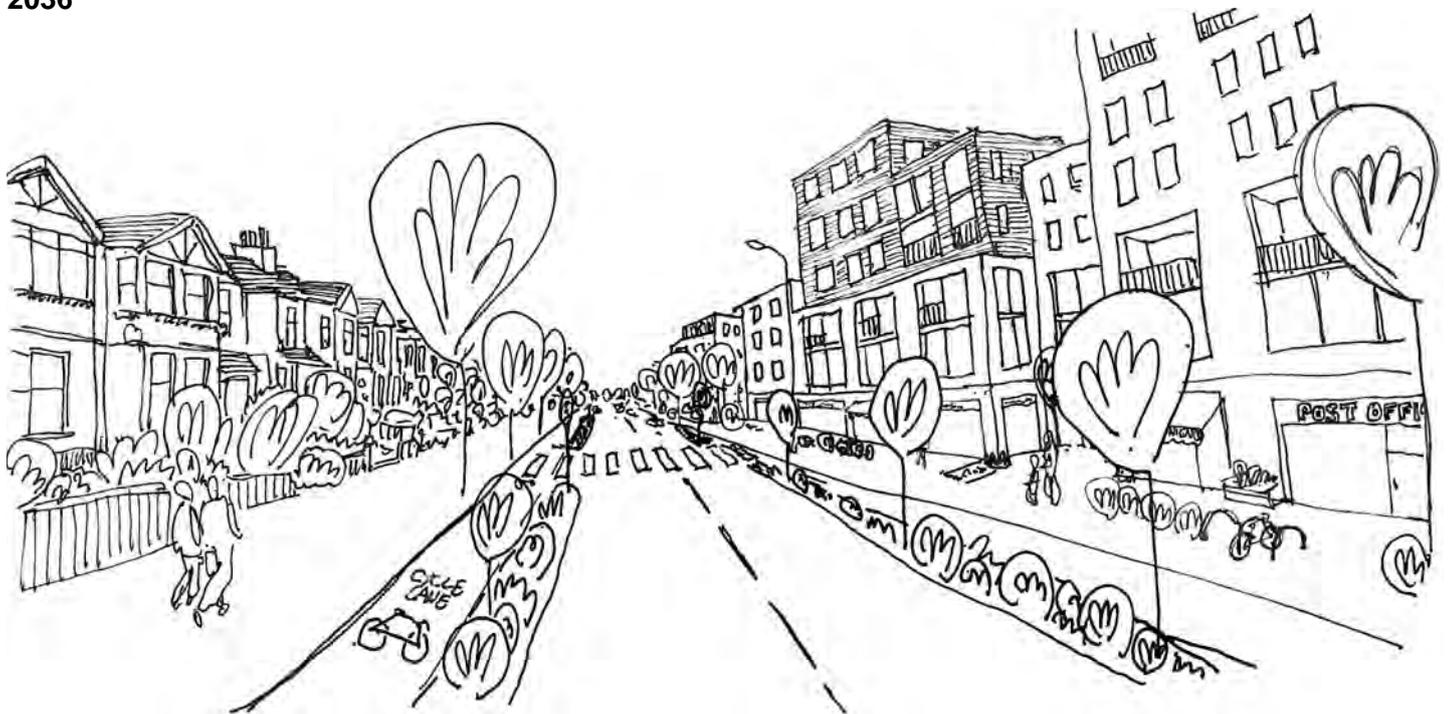


Figure 3.2f: **2036** - New developments of additional height provide an active road frontage, giving it a human scale and reducing the dominance of the road. Through interventions by the Council, or where relevant TfL, the thoroughfare is maintained but adjusted to provide public realm improvements to help prioritise pedestrian movement.

KENLEY

3.3 INTENT OF THE KENLEY INTENSIFICATION AREA:

3.3.1 Redevelopment and development in the area (as designated in the Croydon Local Plan) should seek to provide additional housing and support an associated increase in population. This is proposed to be achieved through a variety of dwelling types and a revitalisation of local businesses and services along Godstone Road, providing long-term benefit to the community. Developments in Kenley should seek to maintain the leafy character of the area with increased focus around a regenerated village centre. The shopping parade, train station, church, nursery, GP surgery and memorial hall should be supported and improved as necessary to continue to provide important community services.



Figure 3.4a: Station Road.



Figure 3.4b: Corner of Kenley Lane and Welcomes Road.

3.4 AREA APPRAISAL

3.4.1 The area around Kenley Station identified for focussed intensification is characterised by predominantly scattered houses on large plots and overall has a green and leafy feel with a variety of building forms separated from the street. The area south of Kenley Lane, including Hayes Lane and Welcomes Road, a private road, is comprised of predominantly detached homes on relatively large plots. These plots typically include off-street parking by way of a garage and driveway. The topography of Hayes Lane is quite steep, with a significant slope further south into the intensification area. These residential plots back onto, and subsequently overlook the residential plots on Welcomes Road. Kenley Lane is also comprised of detached homes on large plots and runs parallel to the railway station before heading south and branching into Welcomes Road.

3.4.2 The western part of the Intensification Area has a largely suburban feel and includes Park Road and Oaklands which contains predominantly medium rise blocks with associated grounds as well as on-street parking and localised green space. Part of Oaklands is designated as a Site of Nature Conservation Importance; any development on this site must take account of this.

3.4.3 The area north of the train line includes some medium rise blocks with associated grounds along with terraced houses and cottages and a small strip of retail uses on Godstone Road. This is opposite the Riddlesdown greenbelt land, which provides significant recreational amenity to the area, including the Kenley Panorama. Any development

proposal should seek to protect and enhance this panorama.

3.4.4 The area is reasonably well accessed by public transport, including buses, and is walkable from Kenley train station. Public transport in the area is expected to improve as a result of the South Croydon Bus Review and improvements to the Brighton Main Line in the East Croydon area. There are however a number of road safety issues that result from local narrow lanes which lack pavements, along with gradients, blind corners and the humpback bridge over the railway. It is noted that the A22 is subject to a current TfL improvement proposal that seeks to address issues resulting from traffic, lack of pedestrian crossing, car parking aside the road and the junction with Hayes Lane. It is important that development seeks to reduce car reliance and there is the potential to introduce schemes, such as a Home Zone or Quiet Lane, that prioritise pedestrians. The safety of the lanes may also be improved by the provision of lighting.

3.4.5 There is an existing GP surgery, local schools, the Kenley Memorial Hall and local church which all contribute to the community and character of the area. The existing parade of shops also provides focus to the community and development should seek to enhance this offering.

3.4.6 Development in Kenley should seek to reduce flood risk as the area is prone to flooding with Station Road and Godstone Road being within Flood Zone 3. Any development proposals within the flood zone should refer to Policy DM25 and Table 8.1 of the Croydon Local Plan which require sequential and exception tests.

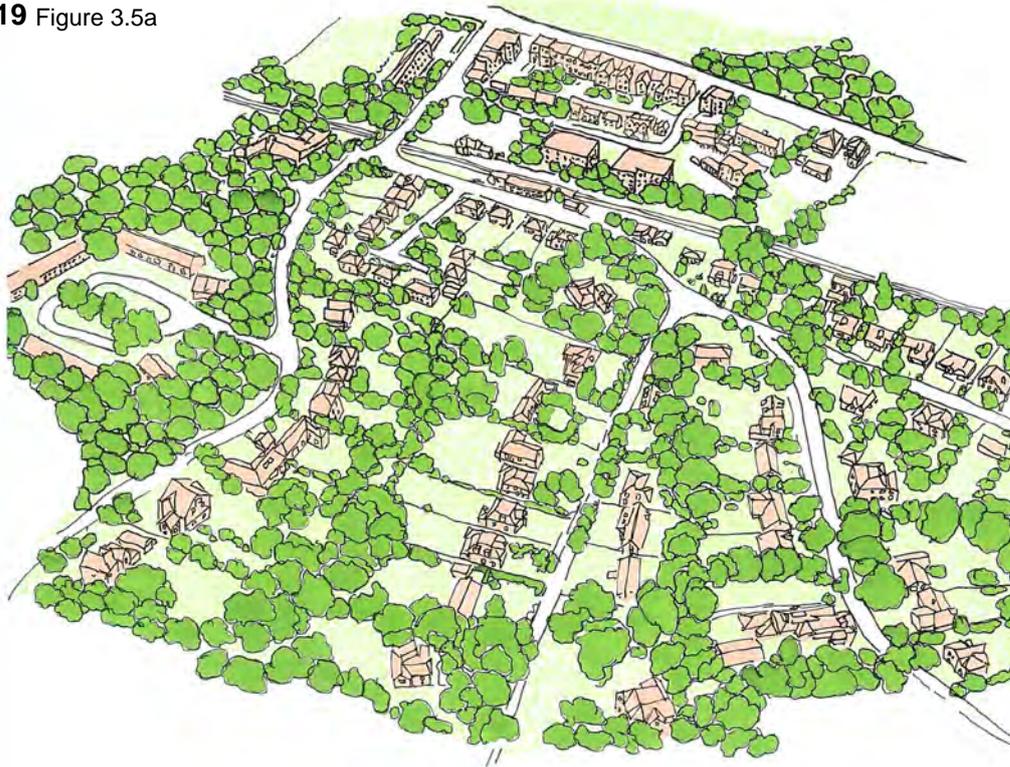


Figure 3.4d: Map with boundary of Kenley Intensification Area (As designated in the Croydon Local Plan).

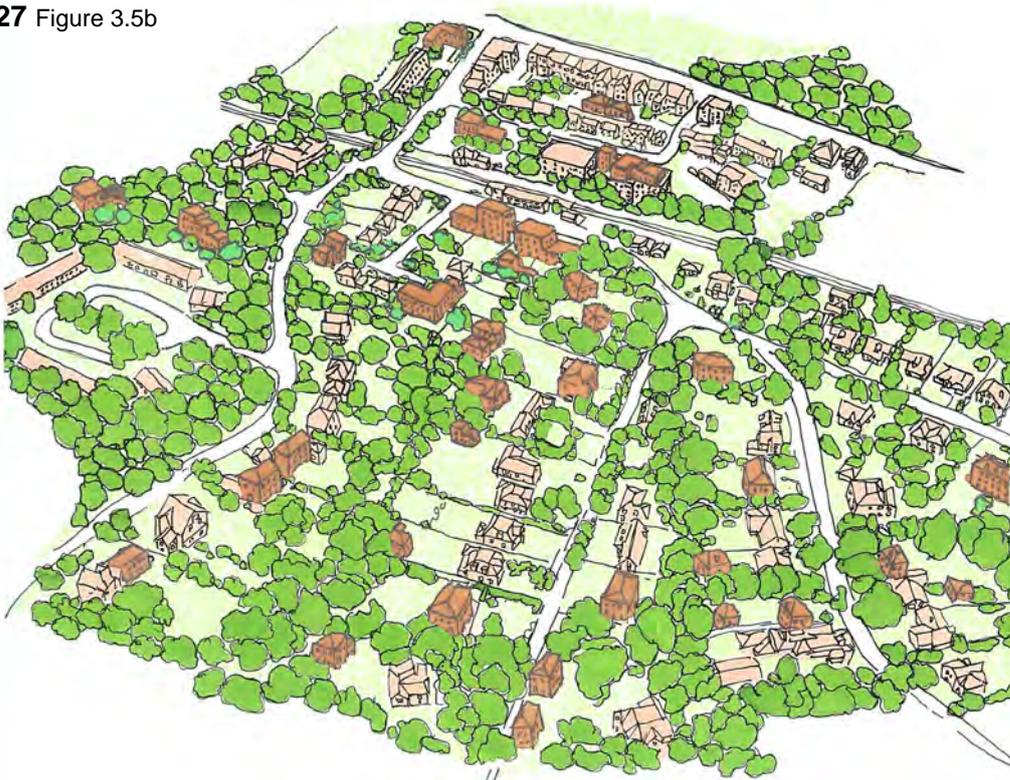
3.5 POTENTIAL DEVELOPMENT SCENARIO (KENLEY)

3.5.1 The scenario described in the following images is indicative and describes one potential way in which the area may be developed. Proposals within the area will be subject to consideration against the Croydon Local Plan, London Plan and this guidance document.

2019 Figure 3.5a



2027 Figure 3.5b



2036 Figure 3.5c



3.6 TYPOLOGY DEVELOPMENT POTENTIAL (KENLEY)

MEDIUM-RISE BLOCKS WITH ASSOCIATED GROUNDS:

- Existing blocks of flats may be redeveloped or extended to provide up to 6 storeys in height where possible.
- Garages associated with these flats present opportunities for development to the same height as the blocks of flats themselves, where this would not unreasonably impact existing residents. Where necessary, garages at ground level may be maintained with accommodation provided above.
- A parking survey will be required to show that the loss of garage parking would not result in a negative impact on parking stress in adjacent roads. If this survey suggests that there will be an impact then the developer will be required to enter into a legal agreement restricting future occupiers from applying for an on street parking permit (Refer to Policy DM30(a) of the Croydon Local Plan). If the site is outside a controlled parking zone then the development will be expected to accommodate parking on site.
- Proposals for existing low & medium-rise blocks should seek to minimise the amount of hardstanding land onsite and introduce a greater amount of landscaping to prioritise pedestrians over vehicles.

AREAS OF TERRACED HOUSES, COTTAGES & COMPACT HOUSES:

- These housing typologies should be preserved to maintain the character of the area. There may be scope for additional accommodation within roofs, with dormer windows to the front and box extensions to the rear roofs.
- In some instances, these typologies may allow for mews style development to occur.

AREAS WITH SUBURBAN SHOPPING & LINEAR INFRASTRUCTURE:

- The retail and service function of Kenley Neighbourhood Centre should be maintained and enhanced with greater provision of local amenities. The public realm may benefit from both soft and hard landscaping improvements, along with rejuvenated shopfronts and the provision of outdoor seating associated with cafes and restaurants.
- Accommodation above shops is encouraged and the conversion of roof spaces into acceptable habitable rooms is supported, where it does not have a negative impact on the operation and viability of retail units or other employment functions. There may be scope for additional accommodation within roofs, with smaller dormer windows to the front and larger box extensions to rear roofs.
- Car parks may present some development opportunity to provide mixed-use schemes, provided the required quantum of parking is maintained (Refer to Policy DM30 and DM31 of the Croydon Local Plan).
- Social infrastructure and community services should be

accommodated as part of the sustainable growth of the area.

SCATTERED & DETACHED HOMES:

- Developments of 4 storeys will generally be acceptable.
- Smaller plots may provide opportunities to merge with neighbouring sites to form large, comprehensive and coherent development sites; this approach would be encouraged to aid the delivery of affordable housing. Where applicable, these must meet the affordable housing requirements in accordance with Policy SP2.4 of the Croydon Local Plan.
- Where plots are subdivided to create rear garden development, these applicants should consider the development potential of the neighbouring rear gardens and the potential to create a larger site with one comprehensive development proposal. Where there is neighbouring development potential but sites do not come forward in one application, the proposed development should be designed to ensure future access can be accommodated from the access route to the first rear garden development⁴².
- Development proposals must consider the topography carefully to ensure appropriate access and minimise the use of retaining walls (Refer to Section 2.35 for guidance).
- These plots currently provide significant landscape amenity and contribute to the biodiversity of the area. As such the significant loss of landscaping will not be accepted and must be balanced with re-provision of

⁴² Such approvals may be subject to conditions to secure this.

high quality mature planting of native species which will support the local ecology and should be demonstrated on plans provided as part of the development application. Applicants should refer to Sections 2.32-2.36 in Chapter 2.

- Developments accessing onto narrow lanes without pavements should provide a 1.5m buffer strip along the front of the site directly adjacent to the road, allowing greater space for pedestrians, cyclists and passing vehicles. This area should not be planted with shrubs or trees

or enclosed from the road, and may function best as a grass verge or gravelled area. This may require a reworking of landscaping to the front of properties to bring the boundary treatment away from the road. Any lost planting should be reprovided within the scheme.

- Where individual plots are developed into multiple units or there is a loss of parking/ garage block, the Council may require a parking survey to show that it will not result in a negative impact on parking stress in adjacent roads. If there is deemed to be a

negative impact on parking stress then the Council may seek to minimise the overall impact of parking demand on the adjacent roads by requiring the developer to enter into a legal agreement restricting future occupiers from applying for an on street parking permit in the Controlled Parking Zone (CPZ), as per Policy DM30a of the Croydon Local Plan in areas of PTAL 4 and above or in areas of parking stress. If there is evidence of parking stress and the site is outside a CPZ then the development will be expected to accommodate parking on site.



Figure 3.6a: Hayes Lane.

FORESTDALE

3.7 INTENT OF THE FORESTDALE INTENSIFICATION AREA:

3.7.1 The area around Forestdale Neighbourhood Centre (as designated in the Croydon Local Plan) provides opportunity for intensification and revitalisation to create a better public realm surrounded by shops and services, to support new and existing homes. By anchoring development around the existing neighbourhood centre and the Forestdale Centre on Selsdon Park Road, there is an opportunity to enhance the suburban village heart to service greater development. Development should seek to maximise underutilised land to deliver an increased housing density with a suburban feel.



Figure 3.8a The Forestdale Centre.



Figure 3.8b: Shopping Parade.

3.8 AREA APPRAISAL

3.8.1 The area around Forestdale Neighbourhood Centre identified for intensification is a mix of character typologies supported by small-scale suburban shopping areas. Gravel Hill and the eastern side of Selsdon Park Road are typified by semi-detached houses. Whilst the topography along Gravel Hill nearest to the roundabout is gentle, the semi-detached homes along Selsdon Park Road are on plots which slope away from the road, providing opportunities to use the topography to maximise development.

3.8.2 The existing Shopping Parade on Selsdon Park Road is set back from the dual carriageway, with a slip-lane for access. The ground level retail includes residential accommodation above, with large backlands accessed via a rear lane. The Forestdale Centre, located to the south of the road junction between Selsdon Park Road and Featherbed Lane, provides further retail offer. These two shopping areas are disconnected and dominated by the dual carriageway and car parking. Through revitalising the public realm and delivering mixed-use schemes, there is an opportunity to create a heart in the area that prioritises pedestrians



Figure 3.8c: Selsdon Park Road.

and encourages the wider community to utilise its services.

3.8.3 There are two (2) fuel stations within the Intensification Area, servicing different directions of traffic. The service station at the Selsdon Park Road roundabout occupies a prominent corner and road frontage, separating the semi-detached homes on Gravel Hill from the terraced houses south along the main road. This corner could better define the street and contribute to a developing character for the area. The Esso service station on the southern side of Selsdon Park Road creates a separation between the neighbourhood centre retail, including The Forestdale Arms, and the medium rise blocks to the west along the main road. These blocks of flats occupy large associated grounds, providing potential for increased development in terms of density and intensity that could deliver greater definition to the main road.

3.8.4 The plots on the eastern side of Featherbed Lane within the intensification area include a variety of houses and services with an inconsistent typology. Accessed by a separate carriageway, this area provides an opportunity to allow connections into the Metropolitan Greenbelt for recreational use. It will be important to strengthen pedestrian links from the Neighbourhood Centre across Featherbed Lane to this location.

3.8.5 The area is served by a number of schools, along with a GP surgery, three bus routes and access to the tram from Gravel Hill. Improvements to infrastructure are set out in the Croydon Infrastructure Delivery Plan.



Figure 3.8d: Map with boundary of the Forestdale Intensification Area (As designated in the Croydon Local Plan).

3.9 POTENTIAL DEVELOPMENT SCENARIO (FORESTDALE)

3.9.1 The scenario described in the following images is indicative and describes one potential way in which the area may be developed. Proposals within the area will be subject to consideration against the Croydon Local Plan, London Plan and this guidance document.

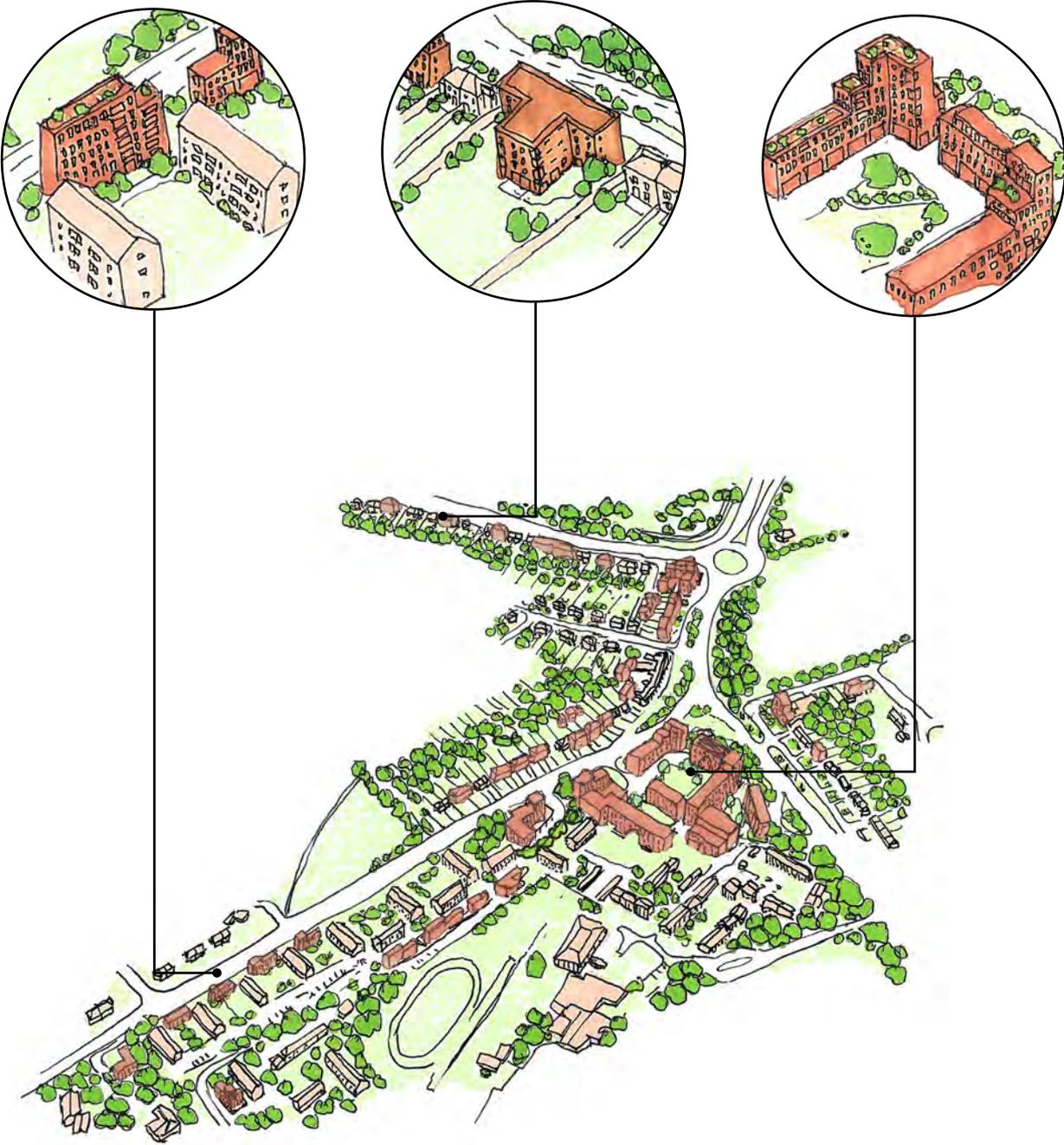
2019 Figure 3.9a



2027 Figure 3.9b



2036 Figure 3.9c



3.10 TYPOLOGY DEVELOPMENT POTENTIAL (FORESTDALE)

SEMI-DETACHED HOUSES:

- Pairs of semi-detached houses may be developed together to provide large developments of flats, maisonettes or terraces. These should seek to provide up to 4 storeys of accommodation, one of which is accommodated in the roof.
- Where possible, proposals may seek to amalgamate 4 or 6 houses (i.e. 2 or 3 pairs of semi-detached houses) in a row to create larger developments which respond to topography. Applicants should refer to the guidance for building across boundaries where this is proposed (Refer to Section 2.15 for guidance).
- Where there is an inconsistent ridge line and change in topography, additional height may be accommodated and, where possible, provide basements and undercroft parking to utilise the slope of the land. Large retaining walls should be avoided. Ramps leading from undercroft parking onto the public highway must be well set back from the back of the footway and the ramp levelled off before a vehicle gets close to the public highway to ensure clear visibility and the safety of pedestrians on the footway at all times.
- Large gardens provide opportunities for rear garden development. These can be accessed by driveways created along the side of the existing houses, particularly where neighbouring properties have similar potential. In these circumstances, the

proposed development should be designed to ensure future access can be accommodated from the access route to the first rear garden development.

TERRACED HOUSES AND COTTAGES:

- Where a set of terraced houses can be redeveloped comprehensively, there is potential for increased development of up to 4 storeys of accommodation, one of which is accommodated in the roof.
- Terraced houses also provide opportunities to create back land developments up to 2 storeys where garages to the rear exist.
- Where individual plots are developed into multiple units, the Council will seek to minimise the overall impact of parking demand on the adjacent roads by restricting permission to apply for on street permits in controlled parking zones.

SHOPPING PARADE (NORTH OF SELSDON PARK ROAD):

- The Shopping Parade should be maintained in terms of use and appearance, without inhibiting the potential for public realm improvements including better connections across to the Neighbourhood Centre and green belt.
- Development should maintain the retail units and encourage additional residential storeys through the provision of small dormer windows to the front roof and box extensions to the rear roofs.
- Underutilised land to the rear of shopping parades may be considered for back land developments of up to

2 storeys, where it does not compromise the functionality of the Shopping Parade or centre.

NEIGHBOURHOOD CENTRE:

- The existing Neighbourhood Centre provides an opportunity to create a central village that includes residential provision, in the form of ground floor retail with 4-5 storeys of residential accommodation above.
- Development should help to define the public realm and strengthen the positive characteristic of the neighbourhood centre, providing a stronger frontage to Selsdon Park Road and Featherbed Lane, and establishing pedestrian connections to the shopping facilities and houses across these roads.
- Where car parking is provided, it should not dominate the environment and should be discreetly located, including in basement car parking where possible.
- Development should safeguard or re-provide pedestrian routes into the Neighbourhood Centre, ensuring they are well overlooked, with good surfaces and lighting, to ensure safety.

MEDIUM RISE BLOCKS WITH ASSOCIATED GROUNDS:

- Land associated with these blocks provide amenity space along with opportunities for intensified development to create a stronger frontage along Selsdon Park Road and a better connection into the neighbourhood centre. Any development should respond to the setting of the existing blocks and not significantly reduce the amount of existing amenity space.

- Garage sites may be considered for development but should seek to minimise the substantial loss of garden areas.
- Where there is a loss of garages, the Council may require a parking survey to show that it will not result in a negative impact on parking stress in adjacent roads. If there is deemed to be a negative impact on parking

stress then the Council may seek to minimise the overall impact of parking demand on the adjacent roads by requiring the developer to enter into a legal agreement restricting future occupiers from applying for an on street parking permit in the Controlled Parking Zone (CPZ), as per Policy DM30a of the Croydon Local Plan, in areas of PTAL 4 and above or in areas of parking stress.

- If there is evidence of parking stress and the site is outside a CPZ then the development will be expected to accommodate parking on site.
- Development must be considerate of adjoining uses and ensure issues of overlooking and safety are mitigated.



Figure 3.10a: View across part of the Forestdale Intensification Area.

BRIGHTON ROAD

3.11 INTENT OF THE BRIGHTON ROAD (SANDERSTEAD ROAD) INTENSIFICATION AREA:

3.11.1 Developments in the Brighton Road (Sanderstead Road) Local Centre (as designated in the Croydon Local Plan) should seek to develop the sense of place where it is diminished by the busy nature of Brighton Road and under-utilised plots. Redevelopment in the Intensification Area should deliver increased housing density, supported by an active and vibrant local centre of mixed uses and recreation areas in conjunction with the existing recreation ground. Development should encourage public transport connections, utilising the opportunity to deliver developments of greater height with active frontages along the main roads.



Figure 3.12a: Vacant buildings and commercial uses along Brighton Road.



Figure 3.12b: Terraced houses along Purley Road.

3.12 AREA APPRAISAL

3.12.1 The Brighton Road Focused Intensification Area is a mix of residential, local retail, light industrial and car parking. The houses along Brighton Road are predominantly close-knit, Victorian, semi-detached homes, interwoven with other typologies and scales of development. Away from Brighton Road, the area is characterised by low density residential development on suburban streets, intermixed with small cafes, shops and community uses.

3.12.2 The upper half of Brighton Road within the Intensification Area is characterised by clusters of denser, high street parades which stretch along a large length of Brighton Road. This is at the heart of the Local Centre, where Brighton Road meets Sanderstead Road, and presents significant opportunity to improve the public realm and provide development to create a vibrant local centre that is attractive to the broader community, with the Locally Listed Red Deer Public House building at its centre.

3.12.3 The lower half of Brighton Road contained within the Intensification Area is predominantly low-rise, light industry intermixed with residential housing. While outside the focussed area boundary, the large recreation ground nearby provides an opportunity for intensified development to overlook open space, creating a park and village green at the heart of the Intensification Area.

3.12.4 The area along Sanderstead Road either side of the railway tracks is leafier than the rest of the Intensification Area. The width of the road and presence of cafes and shops

gives the area a village feel. Development should seek to exploit the opportunity presented by under-utilised areas of grounds associated with medium-rise blocks and back lands to provide new housing.

3.12.5 The area is well-connected and easily accessible by public transport providing the opportunity to create new developments of greater density. However, the area has a distinct lack of character with large pockets without a sense of place, and is severed by the existing transport infrastructure. Public realm improvements should seek to establish a unified character to help tie new and existing development together and create connections across the existing road and rail infrastructure. This should help establish an area that is identifiable through character and services, while providing intensified development along the main routes and elsewhere within the area.

3.12.6 There are a number of developments underway or subject to planning permissions in the area, providing a mix of uses that will deliver new homes along with commercial and retail offers.

3.12.7 Development should seek to reduce flood risk recognising the Flood Zone 3 designation running along the Brighton Road. Any development proposals within the flood zone should refer to Policy DM25 and Table 8.1 of the Croydon Local Plan.

3.12.8 The area provides a good level of employment spaces, along with community facilities. Where proposals seek to redevelop these, they must conform to the Croydon Local Plan policies which seek the re-provision of such floorspace.



Figure 3.12c: Map with boundary of Brighton Road Intensification Area (As designated in the Croydon Local Plan).

3.13 POTENTIAL DEVELOPMENT SCENARIO (BRIGHTON ROAD)

3.13.1 The scenario described in the following images is indicative and describes one potential way in which the area may be developed. Proposals within the area will be subject to consideration against the Croydon Local Plan, London Plan and this guidance document.

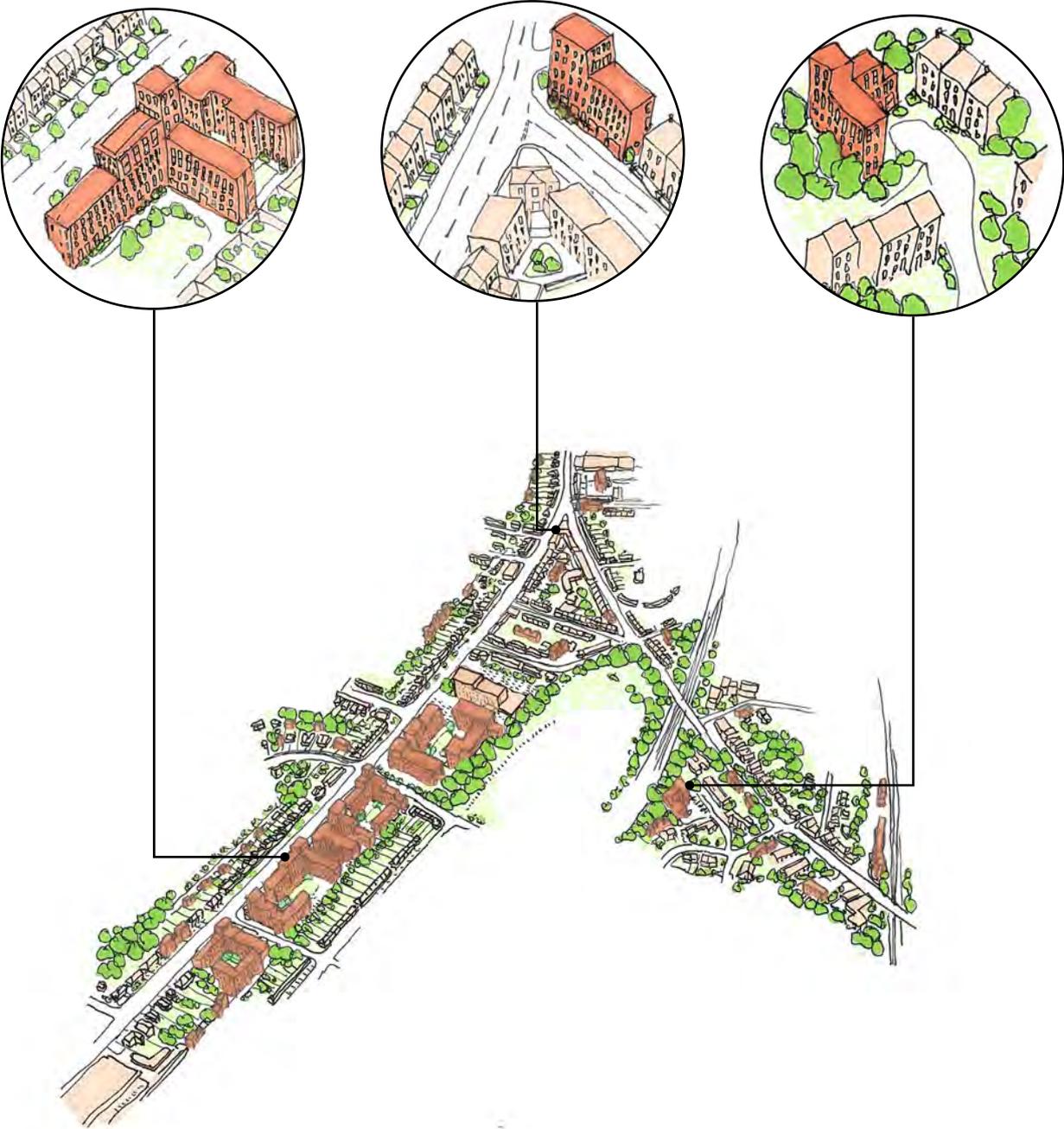
2019 Figure 3.13a



2027 Figure 3.13b



2036 Figure 3.13c



3.14 TYPOLOGY DEVELOPMENT POTENTIAL (BRIGHTON ROAD)

TERRACED HOUSES AND COTTAGES & SEMI-DETACHED HOUSES ALONG BRIGHTON ROAD:

- Where appropriate and safe access via lanes is available, intensification may seek to provide new development within rear gardens (including garages). These should be single storey height with additional accommodation in the roof.
- Where there are back land sites with clearly redundant & un-neighbourly light industrial units and warehouses situated to the rear of existing homes, these may provide opportunities for redevelopment into housing. Mews style houses of up to two (2) storeys are appropriate in these locations and dependent on the setting, there may be potential for additional accommodation within roofs.
- Where there is a consistent ridge line across terraces and pairs of semi-detached houses, the gradual change in height will occur as properties are redeveloped to a greater height.
- Where individual plots are developed into multiple units the Council will seek to minimise the overall impact of parking demand on the adjacent roads by restricting permission to apply for on street permits in controlled parking zones.

SUBURBAN SHOPPING AREAS:

- Suburban shopping areas should maximise opportunities to create vibrant, Neighbourhood Centres with active frontages.
- The retail provision must be retained or re-provided. Where shops are not part of a unified parade or they are in a parade that is single storey, there may be opportunities for redevelopment of up to 4 storeys tall. These should be of a scale that brings definition to the public realm and responds to the context of any of the older or established Shopping Parades.
- Where existing parades of 2 or more storeys exist, these should be retained or reprovided. Where possible, spaces above shops may be converted into residential units⁴³, where it does not compromise the functionality of the Shopping Parade or centre. It may be more beneficial for parades to be redeveloped to a greater height to provide additional accommodation above. This should range between 4-6 storeys depending on the setting.

UNDERUTILISED LARGER BUILDINGS⁴⁴:

- Large, underutilised sites provide potential for the creation of mixed-use developments. These may include active frontages along Brighton Road, with associated public realm improvements.
- Development should seek

⁴³ Conversions from retail to residential must meet the requirements of the relevant policies of the Croydon Local Plan.

⁴⁴ Where not an allocated site in the Croydon Local Plan 2018 and where development is in line with Policy SP3.2 regarding the retention and redevelopment of land and premises relating to industrial/employment activity.

to significantly intensify the area through the development of flats and increased heights. The height of new development should vary to respond to the context and streetscene. Heights should therefore vary from 3 to 6 storeys. Development facing onto Brighton Road or South Croydon Recreation Ground may seek to be up to 6 storeys tall.

- Prioritised pedestrian spaces should be provided within the development plots that are open to the public and allow for connections to the park.
- The provision of family accommodation close to parks and open space is encouraged.

MEDIUM RISE BLOCKS WITH ASSOCIATED GROUNDS:

- Larger sites provide infill development opportunities, including redevelopments of garage blocks. Infill development should be of a massing to allow the open character of these sites to be maintained.
- Garage blocks may be redeveloped to the same height of the host blocks, provided there would be no unreasonable impact on access to light on neighbouring properties.
- Where there is a loss of parking/ garage block, the Council may require a parking survey to show that it will not result in a negative impact on parking stress in adjacent roads. If there is deemed to be a negative impact on parking stress then the Council may seek to minimise the overall

impact of parking demand on the adjacent roads by requiring the developer to enter into a legal agreement restricting future occupiers from applying for an on street parking permit

in the Controlled Parking Zone (CPZ), as per Policy DM30a of the Croydon Local Plan, in areas of PTAL 4 and above or in areas of parking stress. If there is evidence of parking

stress and the site is outside a CPZ then the development will be expected to accommodate parking on site.



Figure 3.14a: Brighton Road.

SHIRLEY

3.15 INTENT OF THE SHIRLEY INTENSIFICATION AREA:

3.15.1 Developments in Shirley (as designated in the Croydon Local Plan) should seek to enhance the Local Centre and further establish the neighbourhood characteristics of the area. Redevelopments should seek to provide an increased density in housing through varying development types and an uplift along Wickham Road to enrich the existing amenities, providing lasting growth to the area as a Local and Neighbourhood Centre. The neighbourhood feel along Wickham Road should be encouraged further west, with improvements to the East-West route leading towards Central Croydon and associated infrastructure along Shirley Road allowing land to be unlocked for development and to improve the public realm.



Figure 3.16a: Shirley Road.

3.16 AREA APPRAISAL

3.16.1 The area defined for focussed intensification in Shirley is predominantly residential-focus intertwined with Local and Neighbourhood Centre services.

3.16.2 The area along Addiscombe Road is identified by semi-detached homes to the north, with detached homes on larger plots on the southern side of the road, as well as the Shirley Park Golf Clubhouse. The roundabout at Shirley and Addiscombe Roads is bordered by a successful parade of independent shops that provide a useful service to the community at this key intersection. Denser development exists at the northern end of the section of Shirley Road within the Intensification Area with some terraced houses, cottages and compact houses on relatively small plots. At the southern end, Shirley Road rises up and dominates the environment, with semi-detached homes on one side separated from the Trinity School of John Whitgift by dual-carriageway and associated slipways.

3.16.3 The Wickham Road portion of the Intensification Area includes Locally Listed Shirley Methodist Church, a mix of semi-detached



Figure 3.16b: A232 Dual Carriageway.

houses and medium rise blocks and Shirley Parish Hall. Importantly, the existing retail strip on Wickham Road is not included in the Area of Focussed Intensification identified for development. Whilst this portion of retail land separates the defined area, the eastern side of Wickham Road is included which is typified by semi-detached bungalows, leading to small scale retail and industry, along with Shirley Library. The number of local community spaces will be important to the continued success of the area and development should seek to enhance these offers.

3.16.4 The Intensification Area as a whole is severed by the dual carriageway road. Creating better pedestrian and cycle crossings is crucial to providing a people focussed link between the Shirley Road Neighbourhood Centre and Shirley Local Centre. Where possible, and as reliance on private car ownership reduces in line with national trends, there may be future opportunity to reduce the width of the road.

3.16.5 There are 6 bus routes that serve the area and there is the potential for the area to provide an improved connection from the east of the borough, creating a gateway to the Croydon Metropolitan Centre. This provides opportunities to look at ways to encourage a lower reliance on cars from East to West entering Croydon, making the roadway safer for cyclists and pedestrians. The inclusion of a designated cycle lane each way would allow denser development to occur with lesser car dependency. Improvements to the dual carriageway area provides an opportunity to make a place that is distinctively recognisable and identifiable as a focus within Shirley.



Figure 3.16c: Map with boundary of Shirley Intensification Area (As designated in the Croydon Local Plan).

3.17 POTENTIAL DEVELOPMENT SCENARIO (SHIRLEY)

3.17.1 The scenario described in the following images is indicative and describes one potential way in which the area may be developed. Proposals within the area will be subject to consideration against the Croydon Local Plan, London Plan and this guidance document.

2019 Figure 3.17a



2027 Figure 3.17b



2036 Figure 3.17c



3.18 TYPOLOGY DEVELOPMENT POTENTIAL (SHIRLEY)

AREAS OF SEMI-DETACHED HOMES:

- Pairs of semi-detached houses may be developed together to provide large developments of flats, maisonettes or terraces. These should provide up to 4 storeys of accommodation, one of which is accommodated in the roof.
- Where possible, proposals may seek to amalgamate 4 or 6 houses (i.e. 2 or 3 pairs of semi-detached houses) in a row to create larger developments which respond to topography. Applicants should refer to the guidance for building across boundaries where this is proposed (Refer to Section 2.15 for guidance).
- Semi-detached houses with large gardens may provide opportunities for rear garden development, particularly where neighbouring properties have similar potential. In these circumstances, the proposed development should be designed to ensure future access can be accommodated from the access route to the first rear garden development.

AREAS OF DETACHED HOMES ON RELATIVELY LARGE PLOTS:

- Redevelopment of 2 storey detached properties into small blocks of apartments may be acceptable. These developments should typically be 4 storeys in height. There may be some scope for additional accommodation in the roof space.
- Rear gardens may be subdivided to create new

houses of no more than 2 storeys tall.

AREAS OF LARGE HOMES ON RELATIVELY SMALL PLOTS:

- Development may seek to amalgamate small plots to establish larger development sites. Larger sites may accommodate blocks of flats or townhouses of up to 4 storeys in height where facing the street.
- Only those with the largest gardens may present the opportunity to be subdivided to provide new homes.

TERRACED HOUSES AND COTTAGES & COMPACT HOUSES ON RELATIVELY SMALL PLOTS:

- Standalone houses may present some opportunity for redevelopment into dwellings of up to 3 – 4 storeys tall, depending on the context and impact on the street scene.
- Where suitable access to the rear of a property exists, there may be some opportunity to provide new development within rear gardens (including garages). These should be single storey height with additional accommodation in the roof.

SUBURBAN SHOPPING AREAS:

- Suburban shopping areas should maximise opportunities to create vibrant, neighbourhood centres with active frontages.
- The retail provision must be retained or re-provided.
- Where shops are not part of a unified parade or they are in a parade that is single storey, there may be opportunities for redevelopment up to a

height of 3 storeys. These should be of a scale that brings definition to the public realm and responds to the context of any of the older or established shopping parades.

- Where existing parades of 2 or more storeys exist, these should be retained or reprovided. Where possible, spaces above shops may be converted into residential units⁴⁵, where it does not compromise the functionality of the shopping parade or centre. It may be more beneficial for parades to be redeveloped to a greater height to provide additional accommodation above. This should range between 4-6 storeys depending on the setting.
- Back land to the rear of existing shops may present the opportunity for redevelopment into housing. These may be mews style houses of up to 3 storeys, dependent on the setting and resulting impacts on neighbouring amenity and the streetscene.

INSTITUTIONS WITH ASSOCIATED GROUNDS⁴⁶:

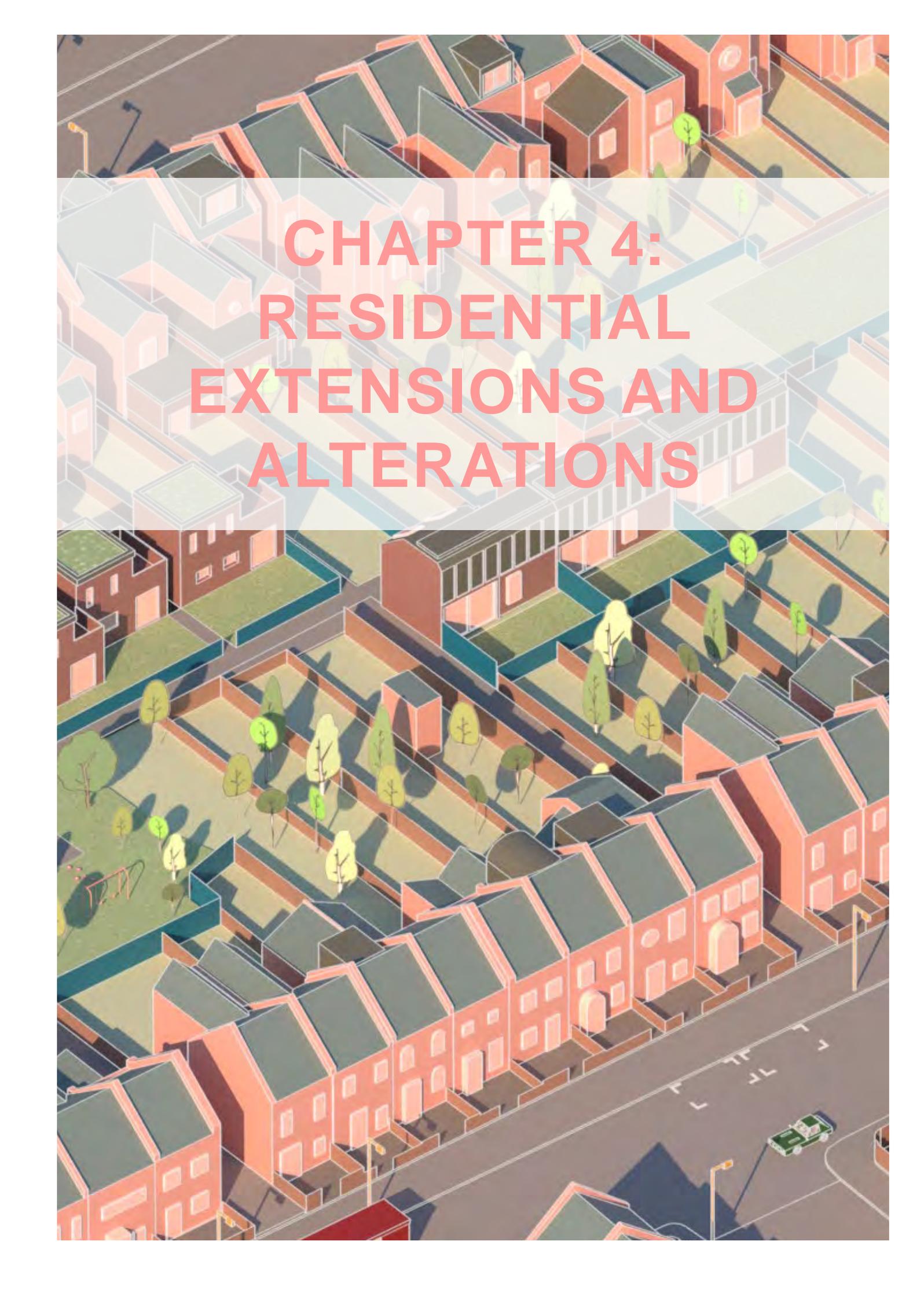
- Larger sites provide opportunities to revise infrastructure provisions to create new mixed-use development potential and increase density.
- Where existing spaces allow, there may be opportunity for infill development.

⁴⁵ Conversions from retail to residential must meet the requirements of the relevant policies of the Croydon Local Plan 2018.

⁴⁶ Where in accordance with Policy SP5 Community Facilities of the Croydon Local Plan 2018.



Figure 3.18a: Wickham Road.

An aerial, isometric illustration of a residential neighborhood. The scene is dominated by red brick buildings with dark grey roofs. The buildings are arranged in a grid-like pattern, with narrow streets and small courtyards. In the center, there are several green spaces with trees and a playground. A road with a white car is visible in the bottom right corner. The overall style is clean and modern, with a focus on urban planning and architecture.

CHAPTER 4: RESIDENTIAL EXTENSIONS AND ALTERATIONS



Figure 4.1a: A semi-detached home with set-back side extensions. (Photo: Ruth Ward)

INTRODUCTION

4.1 EXTENSIONS & ALTERATIONS

4.1.1 Extensions and alterations enable existing housing stock to be improved and evolve for the occupiers. Innovative and creative design solutions for extensions and alterations are encouraged and proposals must demonstrate the design merits of the development. In some circumstances, extensions and alterations may not require planning permission. Where a proposal is deemed to be Permitted Development, applicants should refer to Section 4.6 for further guidance.

4.1.2 Extensions and alterations can significantly change the appearance of a property and, where poorly designed, this can have a detrimental impact on the character and amenity of an area. They can also have significant impacts on neighbouring properties. Any extension should be designed and developed appropriately to ensure that it does not cause a harmful loss of light, visual intrusion or privacy. The scale and appearance of an extension or alteration should also consider the impact on the neighbourhood, and whether it would result in the loss of soft vegetation that contributes significantly to the appearance of the area.



Figure 4.1b: An extension that successfully uses contemporary details and an un-symmetrical roof pitch to add interest to this design by Trehwela Williams. (Photo: Simone Bossi)

DESIGN PRINCIPLES: EXTENSIONS & ALTERATIONS

4.2 RESPOND TO CHARACTER

4.2.1 Developments should consider the character of the area and dwelling to which an extension or alteration is proposed. The built character of an area includes, but is not limited to the size, shape and positioning of buildings, the associated landscaping, materials and details. Extensions and alterations should seek to respond to the character of a dwelling and the existing appearance of the streets. Respond does not mean replicate and the Council will encourage innovative designs that work with the existing character of a building and place. Any proposals which are considered to have a detrimental impact on character will generally be unacceptable.

4.2.2 For further information on how to assess the character of a building or place, applicants should refer to the documents below:

- Detailed information on the characteristics of each area of Croydon is available in the Borough Character Appraisal⁴⁷.
- Detailed information on the characteristic of the predominant housing types within Croydon is available in the Borough Character Typology study⁴⁸.

4.2.3 Where considering proposals that may impact on heritage assets, such as in Conservation Areas or to Listed Buildings, please refer to Heritage guidance in Section 1.4.

4.3 SCALE

4.3.1 Extensions and alterations should generally be of a scale that is subservient to the existing dwelling in accordance with Policy DM10.1 of the Croydon Local Plan. Subservience is required to prevent terracing between and to the rear of existing properties, or to avoid uncharacteristically large additions to the front of a property that would detract from the appearance of the street. Through following the guidance in this chapter (Refer to Sections 4.11 – 4.22) subservience will usually be achieved. However, this should not however stifle or discourage high quality design in terms of form, fenestration, materials and detailing, as set out in Approaches to Design (Refer to Section 4.5).

4.4 SUSTAINABILITY

4.4.1 The environmental impacts and long term sustainability of extensions and alterations is a key consideration in the design of an extension and/or alteration. Proposals for extensions and alterations should seek to integrate materials, insulation, heating, lighting and ventilation systems which minimise energy consumption and improve the environmental performance of the building. This should be considered from the outset of developing a proposal.

4.5 APPROACH TO DESIGN

4.5.1 Extensions and alterations to an existing dwelling should respond to character (Refer to Section 4.2) and be subservient in scale (Refer to Section 4.3), whilst developing a high quality approach to the design in terms of the form, fenestration, materials and detailing. The following two distinct approaches, **Supplementary** or **Innovative**, provide broad design direction to the development of a proposal, however there may be other successful approaches and those outlined here should not stifle creativity in achieving high quality design.

⁴⁷ Available in the Urban Design, Local Character and Heritage section of the local plan evidence at: <https://www.croydon.gov.uk/planningandregeneration/framework>

⁴⁸ Available in the Urban Design, Local Character and Heritage section of the local plan evidence at: <https://www.croydon.gov.uk/planningandregeneration/framework>.

SUPPLEMENTARY

4.5.2 This is the approach that most proposed extensions and alterations are likely to take as it can be easiest to achieve successfully and affordably. A supplementary approach will typically have a form that does not distract from the appearance of the existing house, but may still introduce contemporary elements, such as increased proportions of glazing or new materials. The materials and details should complement the existing house, but do not necessarily need to replicate them and should allow the existing house to maintain its prominence.



Figure 4.5a: A supplementary side extension designed by Selencky Parsons. The form clearly relates to the existing house, but successfully introduces larger windows and combines new materials with brickwork to complement the existing house. (Photo: Andy Matthews)

INNOVATIVE

4.5.3 This approach may be suitable for challenging sites that require a particular design response or where the context provides opportunity to depart from traditional domestic aesthetics. This might be through the use of contemporary materials, unique forms and/or new construction methods. An innovative approach should provide the highest quality design and allow an extension and alteration to be distinguished from, whilst enhancing, the existing dwelling. An innovative approach will require more investment in the design and construction of a proposal due to its bespoke nature.



Figure 4.5b: This innovative extension designed by Alison Brooks Architects enhances the existing dwelling through its contrasting form, use of the highest quality materials and contemporary detailing. (Photo: Paul Riddle)



Figure 4.5c: A series of extensions designed by fourth_space that appear supplementary to the original house by clearly responding to its existing form and materials.

PLANNING CONSIDERATIONS FOR RESIDENTIAL EXTENSIONS AND ALTERATIONS

4.6 PERMITTED DEVELOPMENT

4.6.1 Many proposals for extensions and alterations to a home may be possible under Permitted Development (PD) rights. PD provides rules that allow people to alter existing buildings, and in some circumstances create new buildings, without needing to apply for planning permission. However, the scope of an extension and alteration under PD is limited and technical guidance is available on the Planning Portal⁴⁹.

4.6.2 This guide provides a level of design quality for proposals and therefore those seeking to develop under PD may also find the guidance useful to ensure that all proposals for extensions and alterations contribute positively to the existing dwelling and the character of an area, with limited impact on neighbouring amenity.

4.6.3 Where a PD alteration is pursued, homeowners can obtain a Lawful Development Certificate (LDC) from the Council to demonstrate their project is legal under PD rights. PD rights do not generally apply to flats and are more limited for Listed Buildings and conservation areas. In some areas, an Article 4 Direction has also been put in place to manage change in an area by further restricting PD rights.

4.7 MINOR ALTERATIONS

4.7.1 Applicants are advised to contact Croydon's Planning Department⁵⁰ for minor alterations to determine whether planning permission or Listed Building consent is required, or if other relevant legislation or development restrictions are applicable.

4.7.2 All proposals, including those that do not require planning permission and minor alterations should utilise the Detailed Design for Extensions and Alterations sections 4.23 - 4.27. Further advice should be sought from Council's Pre-application Service⁵¹.

4.8 SHARED PROJECTS

4.8.1 In some circumstances, a joint planning application between neighbours can be beneficial. Where both parties seek to create an extension at the same time, this may provide an opportunity to achieve larger proposals than would normally be acceptable due to the impacts on neighbouring properties. A joint application will be subject to a legal agreement that requires both extensions to be constructed and completed at the same time. Applicants should consider this prior to a submission.

4.9 HOME BUSINESSES

4.9.1 The ability to work remotely is increasingly common, meaning many people use their home as the base for their business. Provided the building remains as a dwelling and the use as a business does not cause disruption to neighbours, planning permission for the change of use may not be required. Where this is the case, planning permission may still be required for the creation of additional space for a home business but this will generally be considered the same as a residential use and should follow the guidance contained within this document. Where a plan to use a home business would result in several employees using the premises and/or it could disturb neighbours, planning permission for change of use may be required. For further advice please contact the Local Planning Authority as part of the Councils formal pre-application service.

4.10 SUBDIVISION

4.10.1 Where proposals seek to subdivide a dwelling to create multiple dwellings, such as the conversion of a house into flats or the subdivision of a rear garden to create a separate dwelling, applicants should refer to the relevant guidance on site layout & servicing and landscaping & outdoor amenity space in the Suburban Residential Development section of this guide and Policy DM10.1 of the Croydon Local Plan.

⁵⁰ Applicants should utilise Council's duty planning officer service. More information is available via: <https://www.croydon.gov.uk/planningandregeneration/duty-planning-officer-service>.

⁵¹ For more information, refer to: <https://www.croydon.gov.uk/planningandregeneration/pre-application-meeting-service>.

⁴⁹ <https://www.planningportal.co.uk/>.

SINGLE STOREY EXTENSIONS

4.11 SINGLE STOREY REAR EXTENSIONS

4.11.1 Single storey rear extensions are not normally visible from the streetscene, so are usually less visually intrusive than side or two-storey rear extensions. However, these extensions can still have an impact on neighbouring amenity including access to sunlight and daylight and outlook. To resolve these potential issues, single storey rear extensions should be designed to ensure:

- That in a terraced or semi-detached property it is no deeper than 3.5m⁵² from the rear elevation of the original dwelling.
- That in a detached dwelling, it is no deeper than 45° (in plan) as measured from the centre of the window of the nearest habitable room in the neighbouring property or 3.5m from the rear elevation of the original dwelling, whichever is greater. In semi-detached dwellings, where there is sufficient separation from neighbouring boundaries the 45° rule can be applied to achieve a deeper footprint than 3.5m (Refer to Figure 4.11b).

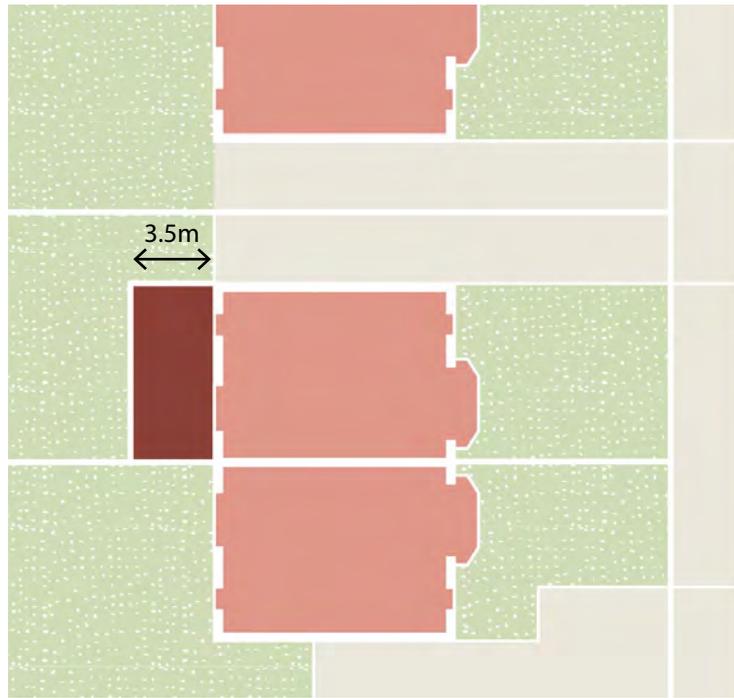


Figure 4.11a: An extension to a terraced house that is no more than 3.5m deep.



Figure 4.11b: An extension to a semi-detached house that is set away from neighbouring boundary, allowing for a deeper extension, up to a maximum of 45° as measured from the centre of the window of the nearest habitable room in the neighbouring properties.

⁵² Permitted development is limited to 3m in all dwellings except detached properties.

- Where there are existing outriggers or extensions, it may be possible to create a dog-legged extension as per Figure 4.11c where the resulting projection of each part of the extension is no more than 3.5m from the respective rear walls.
- Where an existing outrigger or extension is deeper than 3.5m, in some circumstances it may be possible for a new extension to extend up to the depth of the existing outrigger or extension provided there is a compelling design that limits impact on neighbouring amenity.
- The height of a single storey rear extension at its highest point should generally not exceed 4m. The height of a side wall of a single storey that directly abuts a neighbouring boundary will generally need to be less than 4m to minimise impact on neighbouring amenity. Particular consideration needs to be given to the orientation and topography of the site, where this may exacerbate impacts on neighbouring amenity. Refer to Figure 4.11d.
- The detailed design, including specification of materials, windows and doors, should be informed by the guidance on Detailed Design (Refer to Sections 4.23 - 4.27 for guidance).

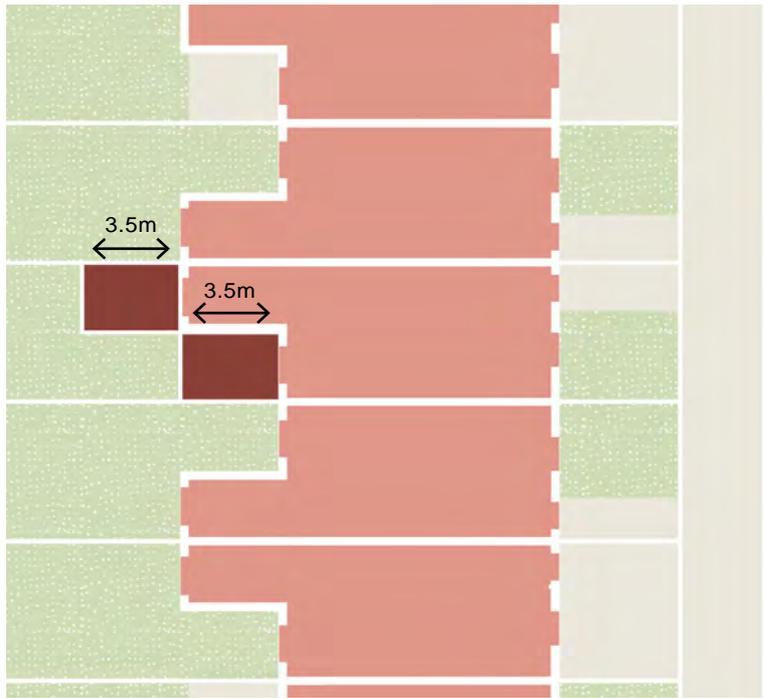


Figure 4.11c: A dog-legged extension where there is an existing outrigger.



Figure 4.11d: Maximum height and relationship with neighbouring boundary.

Height reduced depending on potential impacts to neighbouring amenity

No greater than 4m

4.12 SINGLE STOREY SIDE EXTENSIONS

4.12.1 Side extensions should consider the impact on the appearance of the street. Care is also needed when considering the relationship between any proposed extension and the boundary with neighbouring properties as the separation between properties can provide access routes to the rear of the property and in some locations are part of the character of the area. Depending on the orientation of the neighbouring property, side extensions also have the potential to impact their amenity. To ensure these potential issues are resolved, single storey side extensions should be designed in accordance with the guidance below.

- They may be as deep as the existing house and extend beyond the rear elevation to the distances and in line with the design guidance prescribed in Section 4.11 Single Storey Rear Extensions.
- The height of a wall of an extension that directly abuts a neighbouring boundary should be designed to minimise impact on neighbouring amenity.
- To prevent overlooking of neighbouring properties, windows and doors should normally be placed in the front and rear walls of the extension. If windows are proposed on side walls where they would create issues of overlooking, they should be at high level, non-opening and fitted with obscured glass. Any windows on side elevations should not prejudice the development potential of adjoining land.
- Extensions that are irregular to an existing pattern of buildings along a street will only be acceptable where it can be



Figure 4.12a: A single storey side extension that extends beyond the rear elevation of a property.

Depth beyond rear elevation limited according to guidance on rear extensions

demonstrated they would enhance the appearance of the street and character of the area. In such circumstances the design approach should not upset the balance and proportions of the existing dwelling.

- Where an extension seeks to build beyond the existing front elevation, they should also refer to the guidance on front extensions (Refer to Section 4.14). If they do extend beyond the front building line, applicants are encouraged to combine this with a new or existing porch where applicable.
- The detailed design, including specification of materials, windows and doors, should be informed by the guidance on Detailed Design (Refer to Sections 4.23 - 4.27 for guidance).



Figure 4.12b: A poorly designed single storey side extension that fails to respond to the original dwelling. It has an awkward combination of roof forms and the appearance is further exacerbated by the porch extension, which hasn't been combined with the side extension.

4.13 SINGLE STOREY WRAP-AROUND EXTENSIONS

4.13.1 Wrap-around extensions which seek to extend to the side as well as to the front or rear of an existing house must have regard to impacts on neighbouring amenity and the appearance from the street. Wrap-around extensions should refer to the relevant combination of guidance for side and rear or front extensions.

4.14 SINGLE STOREY FRONT EXTENSIONS AND PORCHES

4.14.1 Front extensions can change the character of the original building and where poorly designed have a negative impact on the appearance of the street; due to their visibility these kind of extensions are most likely to have an impact on the wider streetscene. It is therefore important to invest a high level of design quality in such proposals following the guidance below:

- Extensions that are irregular to an existing pattern of development will only be acceptable where it can be demonstrated they would enhance the appearance of the street and character of the area. This is likely to be challenging in streets with a consistent pattern of development.
- Front extensions must be designed to respond to and enhance the character of the existing dwelling.
- They should generally be no deeper than 1.5m and avoid being full width; overly-wide or deep extensions which



Figure 4.14a: Example of a good side extension wrapping around to incorporate a well-designed porch, successfully integrating with the existing dwelling.

would appear to dominate the appearance of the existing dwelling and fail to enhance character will not be supported.

- The detailed design, including specification of materials, windows and doors, should be informed by the guidance on Detailed Design (Refer to Sections 4.23-4.27 for guidance).

4.14.2 Porches can be added to a house to provide a threshold space between the exterior and interior, whilst adding emphasis to the entrance:

- The scale and design of new porches should respond to the existing dwelling. Care should be taken to preserve the appearance of existing features, such as bay windows and avoid porches that would impact these. The roof design of a porch should be carefully considered to ensure its appropriateness to the existing house.
- Existing porches that are open to the street and are an original



Figure 4.14b: A poor example of two porches that have been built up to and over original bay windows. The design fails to respond to the historic pattern of development and materials are low quality.

feature that form part of a local pattern of development should generally not be enclosed.

- The detailed design, including specification of materials, windows and doors, should be informed by the guidance on Detailed Design (Refer to Sections 4.23 - 4.27 for guidance).

4.15 ROOF DESIGN FOR ALL SINGLE STOREY EXTENSIONS

4.15.1 The shape or form of the roof can have a significant impact on the appearance of an extension, but can also add interest to a design. The design of a roof needs to consider the relationship with the existing house and surrounding context. It is therefore important to consider their appearance and performance as part of the design following the guidance below:

- The design of roofs for all single-storey extensions must not create unreasonable negative impacts on neighbouring amenity. Roof designs that result in excessive visual intrusion and/or the blocking of natural light to neighbouring properties must be avoided.
- Where a pitched roof is proposed, care needs to be taken with how this may relate to windows on the first floor, where the roof meets the outer walls of the existing house. (Refer to Figure 4.15a).
- Where a flat roof is proposed, this should not normally be proposed to be used as a terrace or balcony. This is to protect the privacy of neighbours. However, in some cases it may be possible if it is demonstrated that neighbour's amenity in both directions is not impacted. The introduction of screening devices to help prevent overlooking from terraces or balconies are generally not considered acceptable as these can be detrimental to suburban character.
- Consideration should be given to how rainwater goods will be accommodated into the design



Figure 4.15a: An example of a roof design to a single storey extension by Nimtim Architects that has been designed to ensure it doesn't overlap with the windows above. (Photo: Anna + Tam)

of the roof (Refer to Section 4.24 for guidance).

- Applicants are advised to consider how the roof of an extension can be used to enhance the environmental performance of their home. This may include providing solar panels or a green roof. Any such proposals are encouraged by the Council but should be clearly shown on drawings submitted with the application. The acceptability of such proposals will however have regards to any potential negative impacts on the visual amenity of neighbouring properties or the appearance of the street.
- The detailed design, including specification of materials and rooflights should be informed by the guidance on Detailed Design (Refer to Section 4.23 - 4.27 for guidance).



Figure 4.15b: A side and front extension with a series of different roof forms results in a poorly considered composition that has a negative impact on the streetscene.

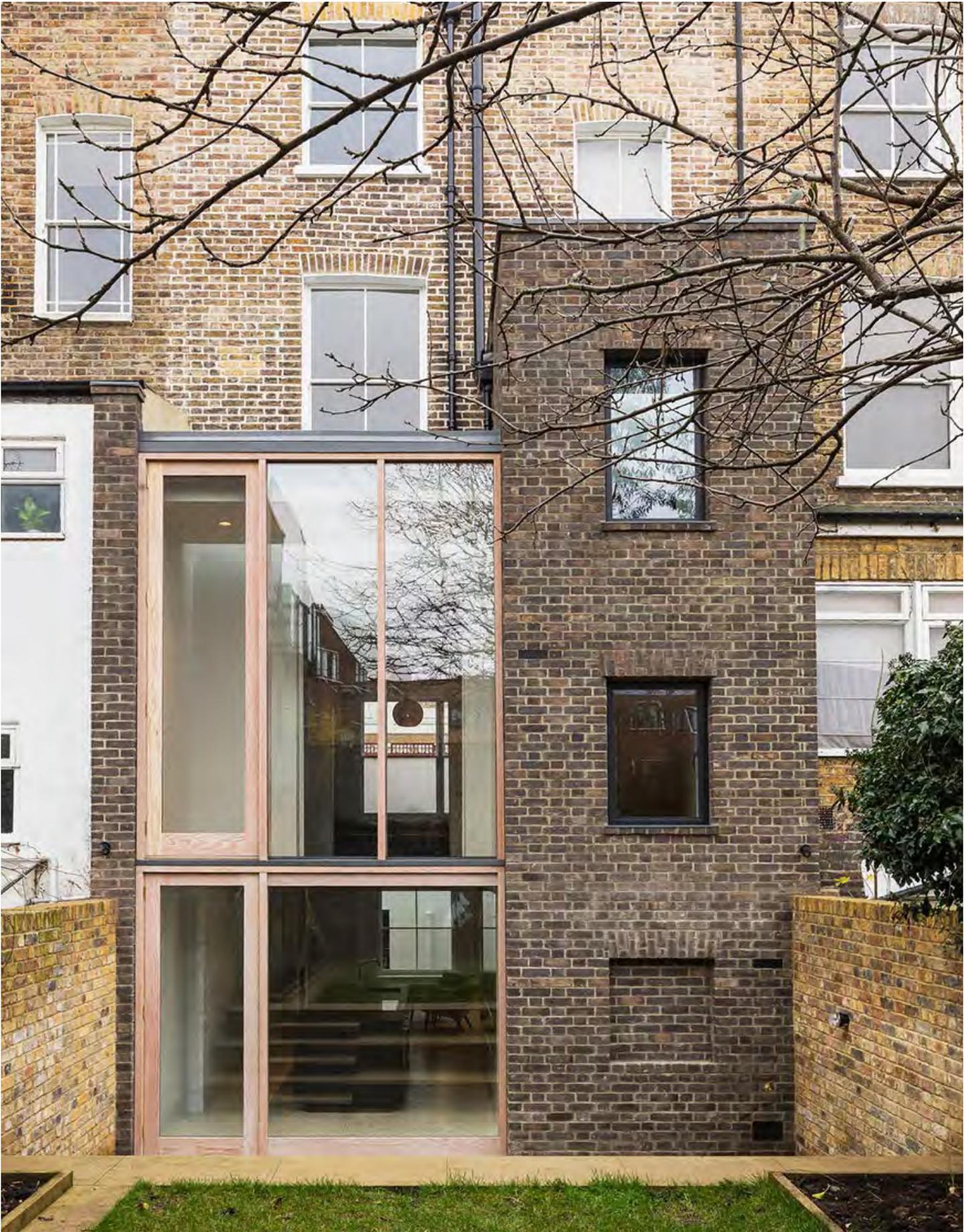


Figure 4.16a: Example of a two-storey rear extension of a terraced house designed by Gundry+Ducker Arch. that infills between existing extensions and outriggers, refer to Section 4.16 for guidance. This scheme also features a well-designed parapet detail to the roof (refer to Section 4.15) and concealed rainwater goods (refer to Section 2.24) giving a clean appearance. (Photo: Andrew Meredith)

TWO-STOREY EXTENSIONS

4.16 TWO-STOREY REAR EXTENSIONS

4.16.1 Two-storey rear extensions are often desirable to create more space within a home, however they need to be carefully designed to avoid negatively impacting neighbouring properties. Proposals for two-storey rear extensions should consider the surrounding context and ensure:

- For all types of housing, they are positioned so that they do not result in unreasonable loss of daylight to habitable rooms in neighbouring properties or result in an unreasonable level of overlooking.
- For terraced houses, they are only proposed where they would be infilling between two existing two-storey extensions or outriggers (Refer to Figure 4.16b) and therefore wouldn't impact on neighbouring amenity. Where this is the case, the extension should be of a height and depth no greater than the existing extensions or outriggers.
- For semi-detached properties, they are located on one side of the rear of the property that does not abut the adjoined property (Refer to Figure 4.16c); or they adjoin the neighbour where it already contains a two-storey rear extension (Refer to Figure 4.16d). They should generally be no wider than half the width of the existing house and no deeper than 45° (in plan) as measured from the nearest habitable room window on neighbouring properties to both sides of the dwelling and should not exceed the eaves and roof ridge line of the existing house.

- For both terraced and semi-detached properties, there may be greater potential to create two-storey rear extensions where a joint scheme comes forward, subject to a legal agreement (Refer to Section 4.8 for guidance).
- For detached properties, they should generally be of a depth no greater than 45° as measured from the nearest habitable room window on neighbouring properties to both sides of the dwelling. They should not normally exceed the eaves and roof ridge line of the existing house.
- The specification of materials, windows and doors is in accordance with the guidance on Detailed Design (Refer to Section 4.23 - 4.27 for guidance).

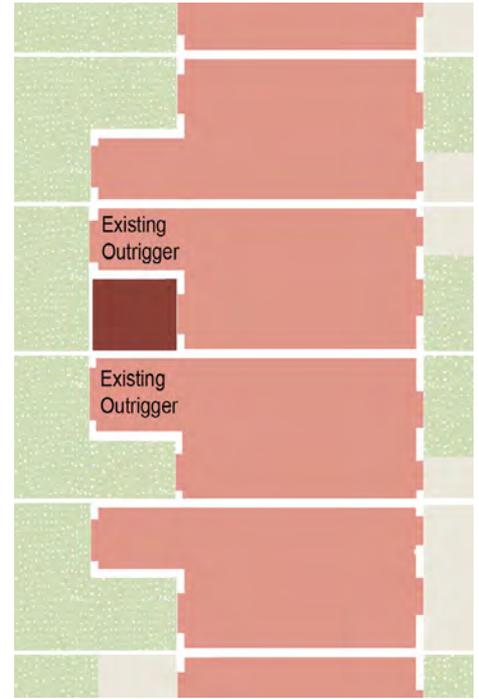


Figure 4.16b: Example of where a two-storey rear extension may be acceptable where it would be infilling between existing extensions / outriggers and would not impact on neighbouring amenity.

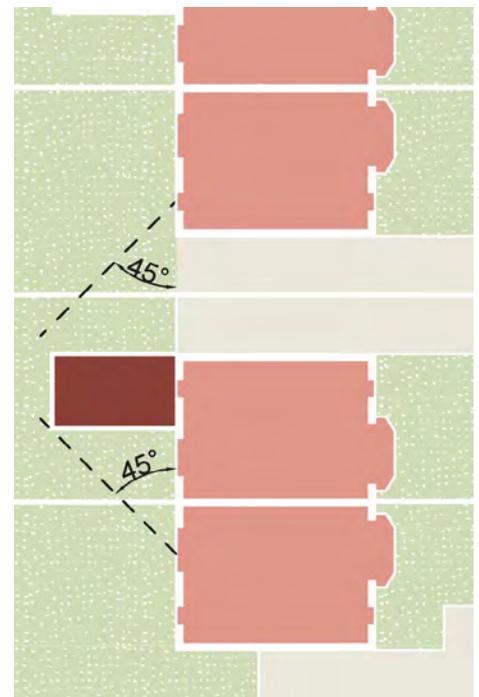


Figure 4.16c: A two-storey extension to a semi-detached house set away from the directly adjoining neighbour.



Figure 4.16d: A two-storey extension proposed to a semi-detached house where a neighbour already has a two-storey extension.



Figure 4.16e: A two-storey extension to a semi-detached house set away from the directly adjoining neighbour.

4.17 TWO-STOREY SIDE EXTENSIONS

4.17.1 Two-storey side extensions are appropriate where space is sufficient and the impacts on the townscape and neighbouring properties are considered.

Two-storey side extensions must consider the surrounding context and ensure:

- They are designed so as not to create an unreasonable impact on access to daylight and overlooking in habitable rooms on neighbouring properties.
- The existing rhythm of the street, including for example characteristic gaps between properties, the symmetry of pairs of semi-detached homes or groups of terraced houses, would not be unreasonably interrupted.
- They do not result in an overly wide or poorly proportioned elevation facing the street. This can usually be avoided by setting the extension back from the existing front elevation; this should be at least 1m at the first floor, while a ground floor setback of approximately 1 brick (215mm) could be provided. In some special circumstances a reduced setback may be allowable and would need to be justified in an application and considered on a case by case basis.
- They do not exceed the eaves and roof ridge line of the existing house.
- The specification of materials, windows and doors is in accordance with the guidance on Detailed Design (Refer to Section 4.23 - 4.27 for guidance).



Figure 4.17a: A good example of a setback at first floor on a two-storey side extension.



Figure 4.17b: A low quality two-storey side extension which is overly dominant, upsetting the balance of this pair of semi-detached homes. The brick neither matches nor distinguishes from the existing and the junction at the eaves is poorly detailed.



Figure 4.17c: Example of a subservient two-storey side extension designed by Selencky Parsons that introduces contemporary elements, such as the windows and their surrounds, to help distinguish the new from the existing. (Photo: Andy Matthews)

4.18 TWO-STOREY FRONT EXTENSIONS

4.18.1 Two-storey front extensions are likely to have a significant impact on the appearance from the street and will be determined on a case-by-case basis.

4.19 TWO-STOREY WRAP-AROUND EXTENSIONS

4.19.1 Two-storey wrap around extensions can introduce a large additional volume and therefore need to be carefully designed to respond to the character of the existing dwelling and neighbouring properties.

4.19.2 Two-storey wrap-around extensions which cover the side and rear or side and front of a dwelling will generally be determined on a case-by-case basis and where they follow a combination of guidance for the applicable extension (Refer to Sections 4.16, 4.17 and 4.18 for guidance).

CORNER PLOTS

4.20 EXTENSION TO HOUSES ON CORNER PLOTS

4.20.1 Corner plots provide opportunities to create large extensions that face onto the return road and in some cases can create a landmark building feature. Their location makes them highly visible from two streets and can provide an opportunity to improve the appearance of an area. Houses on corner plots may have capacity for two-storey extensions that extend to the side or rear. They should be designed to create a positive relationship with the existing dwelling, neighbouring properties and street scene and ensure:

- Where extensions are proposed that would project beyond the rear of the existing dwelling, they follow the guidance on rear extensions (Refer to Sections 4.11 or 4.16 for guidance). Where separation with the neighbours and orientation allows, there may be scope for a deeper extension.
- Where extensions are proposed that would project beyond the side wall of the existing dwelling they follow the guidance on side extensions (Refer to Sections 4.12 or 4.17 for guidance).
- Any projection forward of the building line on the return street is carefully designed as this will be highly visible. This may be resolved through the massing (such as stepping), fenestration or material treatment of the proposal. Views along the return street to the proposed building should be considered.
- The relationship between the roof of the existing property and an extension on a corner is carefully considered. Extensions that result in overbearing end walls, including uncharacteristic gables, will generally not be acceptable.
- The specification of materials, windows and doors is in accordance with the guidance on Detailed Design (Refer to Sections 4.23 - 4.27 for guidance).

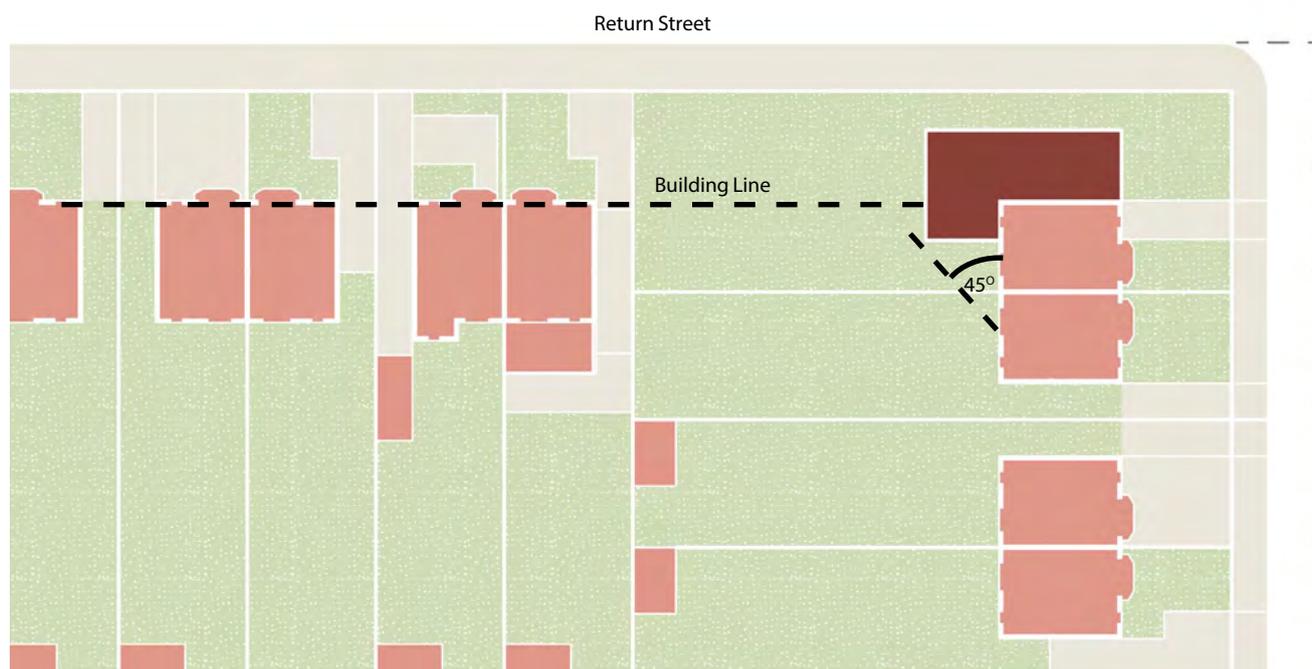


Figure 4.20a: A plan of an acceptable corner plot extension.

ROOFS

4.21 EXTENSIONS & ALTERATIONS TO ROOFS

4.21.1 The use of loft space to provide additional accommodation can often provide more space for relatively little cost, using natural light through the use of skylights. Roof extensions, such as dormer windows or box extensions which project out from the roof slope, should be used where there is a need to enlarge the useable floor space within a loft or where they are more characteristic of the area. Extensions and alterations to roofs should follow the guidance below:

- Ideally be located on the rear elevation of a dwelling to minimise impact on the street.
- May be full-width for mid-terrace houses, but should be set in from the edge of a hipped roof or gable end on end of terrace houses (refer to Figures 4.21b and 4.21g).
- May be no more than two-thirds the width of the existing roof on a semi-detached or detached house, and should be set in from the edge of a hipped roof or gable end (refer to Figures 4.21a and 4.21g).
- Should be no higher than the existing ridge-line.
- Should not wrap around two-sides of a hipped roof unless in special circumstances where it can be justified; this will be judged on a case by case basis.
- Should include generously sized windows that are generally best if positioned to relate to the existing doors and windows on the floor below. Large blank facades on dormers can have an overbearing appearance and will not generally be acceptable.
- If proposing a hip to gable roof extension, should not interrupt

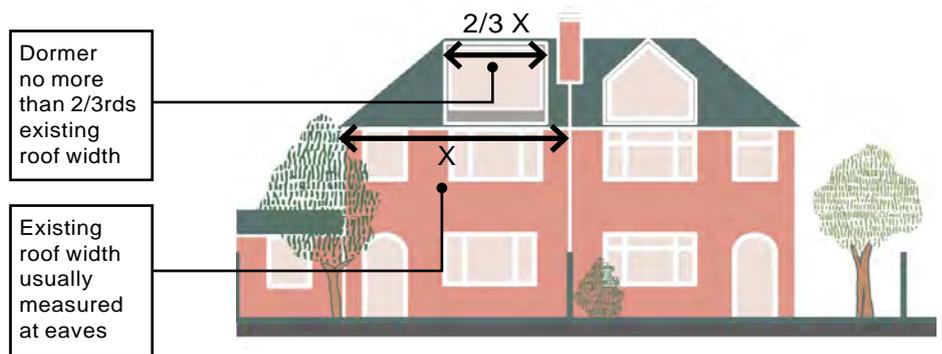


Figure 4.21a: Dormers on a semi-detached or detached house, no more than two thirds the width of the existing roof, and set in from the edge of a hipped roof (or a gable end).

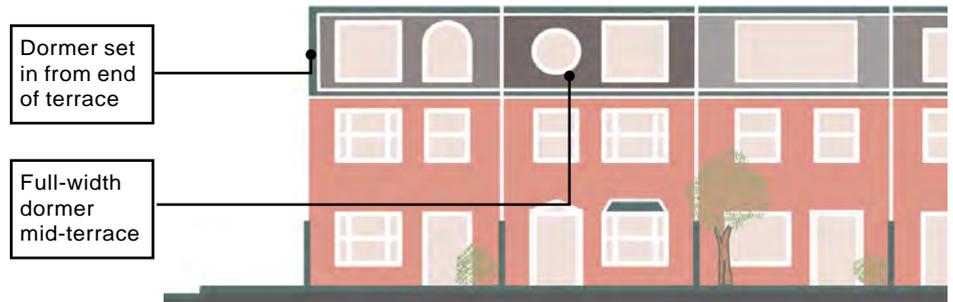


Figure 4.21b: Dormers on a terraced houses that are full-width for mid-terrace houses, but set in from the edge of a gable end (or hipped roof) on the end of terrace house.

the pattern of roof forms visible from the street.

- If proposing a side roof extensions, be no more than two thirds the width of the existing roof and should not interrupt the appearance of the roof when viewed from the street (refer to Figure 4.21e). Habitable room windows in the side elevation facing a neighbouring property would not normally be acceptable if it results in overlooking to habitable rooms or the first 10m of the rear garden of a neighbouring property.
- Choose materials, windows and doors in accordance with the guidance on Detailed Design (Refer to Sections 4.23-4.27 for guidance). This is important for roof extensions due to their visibility, the need to avoid creating an overbearing appearance and the potential to add design interest through materials and detailed design.

4.21.2 It will generally not be acceptable to create dormers on the front of a property. They will only be possible in exceptional circumstances which includes the Areas of Focussed Intensification (Refer to relevant guidance in Chapter 3) or other locations where they would not negatively impact the appearance of the street and not disrupt the rhythm of development along a street. Where this may be possible, they should not be full width or large box dormer, and should generally be setback from the eaves line by a minimum of 0.3m. They should be positioned to be part of the composition of the front elevation, relating to the shape, size, position, and design of the existing doors and windows on the lower floors including space between windows and offsets from side walls. Rooflights may be less disruptive to the street scene and should be considered for front elevations.



Figure 4.21c: A good example of a box dormer on the rear of a terrace house successfully designed by Selencky Parsons to integrate into the existing dwelling; the dormer replicates existing roof tiles, conceals rainwater pipes and aligns the glazing with the windows below. The dormer provides a generous amount of glazing, avoiding an overbearing appearance and benefiting the internal spaces.



Figure 4.21e: Example of where a side roof extension to a house has not been setback from the original roof. In this situation it has compromised the symmetrical form of the original semi-detached houses and therefore negatively impacts the appearance of street.



Figure 4.21f: Example of an inappropriate addition of a front dormer to a mid-terrace house. This addition lacks design merit and breaks the uniformity of the roofs that contributes to the positive characteristics of the street.



Figure 4.21d: The addition of 3 dormers by Threefold Architects that have been sympathetically designed to the existing building and respond to the positioning of the windows below.



Figure 4.21g: Example of where a highly visible roof extension dominates the original building and negatively impacts the appearance of the street. This could have been avoided had the rear box extension been set in from the gable end of the roof to allow the gable and the rear extension to read as two separate elements.

ADDITIONAL STOREYS

4.22 EXTENDING UPWARDS

4.22.1 Where appropriate, an additional storey added across all or any part of a dwelling can be effective for increasing internal floor area, particularly for dwellings with flat roofs. Proposals for additional storeys should ensure:

- They are generally limited to 1 additional storey, except where in exceptional circumstances, such as on larger flat roofs.
- They are generally only applied to detached houses, blocks of flats or on corner plot for any type of house with adequate separation from the boundary of their plot.
- They do not result in

unreasonable loss of light and direct overlooking to habitable rooms or the first 10m of the rear garden in neighbouring properties.

- They are designed to respond to the existing building. In some circumstances, an approach that continues the form, proportions, materials and details of the floor below may be appropriate. This needs to be approached with care to ensure it does not result in an overbearing or poorly proportioned elevations, and presents a risk materials do not quite match and therefore appear to clash. Where this would be the case a setback may be appropriate taking a supplementary or innovative

design approach (Refer to Section 4.5).

- The specification of materials, windows and doors is in accordance with the guidance on Detailed Design (Refer to Sections 4.23 - 4.27 for guidance).

4.22.2 Where additional storeys would result in the creation of new residential units, they should refer to the guidance provided in Chapter 2 with regards to Site Layout & Servicing and Landscaping & Provision of Outdoor Amenity Space.



Figure 4.22a: An example of additional storeys being added to an existing building to provide new homes.

DETAILED DESIGN FOR RESIDENTIAL EXTENSIONS & ALTERATIONS

4.23 DETAILS

4.23.1 The detail incorporated into the design of a proposal will have a significant impact on the finished appearance of an extension or alteration. There are many aspects which should be considered when developing proposals, such as choice of materials, windows & doors, architectural detailing and ancillary items such as flues and rainwater goods.

4.24 RAINWATER GOODS & OTHER ANCILLARY ITEMS

4.24.1 Rainwater goods, such as downpipes, and other ancillary items, such as flues and soil vent pipes, can add clutter to the appearance of a dwelling. The impact on the appearance of a proposal should be considered in the early design stages and should ensure:

- The positioning of rainwater goods, flues, vents and other pipes, are in a discreet location and the number of downpipes is limited to avoid cluttered elevations.
- Pipework does not overhang the boundary of neighbouring properties⁵³. For flat roofs, the introduction of a parapet is often a successful way to contain rainwater collection within the curtilage of the property. Where the roof slopes towards a boundary, proposals should have a wall setback from the boundary to allow for eaves and gutter overhang. A sloped roof should not generally be combined with a parapet (Refer to Figure 4.24c).
- Where for a single storey side extension, consideration is given to future development of a first floor extension. A single storey side extension up to the boundary could limit the design at first floor if eaves and guttering were to extend over the boundary.
- Meter boxes are placed in a discreet location, generally away from the main entrance or where they are not prominent on the front elevation or subterranean where possible.

⁵³ Information about Party Walls and the Party Wall etc. Act 1996 for boundaries of land belonging to two (or more) different owners is available at: https://www.planningportal.co.uk/info/200187/your_responsibilities/40/other_permissions_you_may_require/16

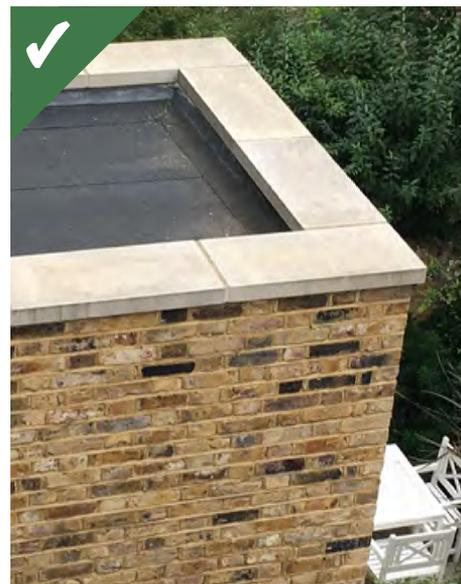


Figure 4.24a: A parapet can be used on a flat roof to remove the need for gutters, creating a less cluttered appearance.



Figure 4.24b: Poorly considered pipework on a side extension, visible from the street.



Figure 4.24c: The combination of a pitched roof with a parapet results in an unattractive appearance.

4.25 WINDOWS AND DOORS

4.25.1 Windows and doors should be designed to avoid poorly proportioned, positioned and detailed openings. The relationship between a proposal and existing openings should also be carefully considered. Designs should ensure:

- Where an extension or alteration meets the existing dwelling, they should generally be at least 215mm (1 brick width) clear of any opening on the existing dwelling.
- The positioning and proportions of windows and doors should avoid an elevation that appears imbalanced or that result in large blank facades that would appear overbearing.
- Consideration is given to whether windows or doors as part of an extension or alteration should be:
 - recessed, semi-recessed or flush with the external envelope;
 - in a symmetrical or asymmetrical composition; or
 - match the proportions of windows in the existing house.
- Where the original doors and windows are characteristic features of the existing dwelling or the area, such as bay windows, they are retained.
- Decorative features to door and window surrounds are retained where possible, particularly where they contribute to the character of a building or area.
- The replacement of an unsympathetic door or window is with one of a design that is characteristic of the original dwelling.
- The choice of material of new windows and door frames is consistent. Where wooden frames are already used, this should be continued unless



Figure 4.25a: A wall with different depths of window and door reveals. Deeper reveals add emphasis and solidity to the appearance of a house.

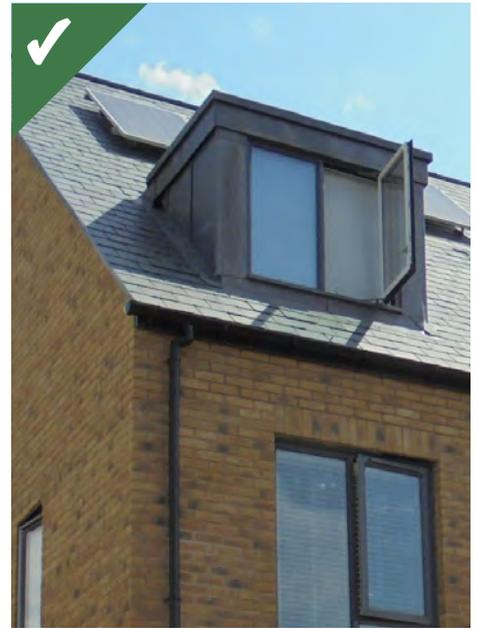


Figure 4.25b: An example of well-designed dormer window, set in from the edges of the roof, with a simple frame that does not draw the attention of the eye.

there is a particular design rationale for introducing a different framing system. Metal frames may be appropriate in contemporary proposals. The quality and appearance of wooden and metal frames tends to be superior to uPVC and are therefore preferable.

- Where the porch is an important part of the original design of a house, these are retained. The enclosure of porches with glazing can interrupt the rhythm of a street and should be avoided. The removal of a porch can result in an under-scaled entrance, diminishing the uniformity of a street where the porch is a feature on all houses.
- Where a porch is added to a dwelling, the building style and impact on the street scene is considered.

4.26 MATERIALS

4.26.1 The choice and use of materials for an extension or alteration can significantly impact the appearance of a dwelling. Choice of materials should consider the neighbouring properties and ensure:

- In areas where there is a strong sense of character through the use of particular materials, extensions and alterations should use materials that respond to this character. Where appropriate, this may allow the introduction of new, high-quality materials, including in historic environments where contemporary materials may be used to offer a contrast to the appearance of traditional materials and enhance the qualities of and provide a clear distinction from the original fabric.
- Materials chosen to match the existing dwelling are carefully chosen to consider the effects of weathering and time. This is crucial where materials need to match the existing.
- The long-term wearing of materials is considered. Materials such as render and wood can wear drastically if poorly detailed and not maintained, particularly if north facing.
- The reuse of materials where



Figure 4.26a: Strong and consistent material palette, features and details contributes to character of a suburban street. (Photo: Ruth Ward)

possible for a repair or extension. Elevations which are visible from the street, including roofs, should be prioritised in the reuse of materials. A mixture of old and new materials is more appropriate on rear-facing elevations, and should ensure that similar colours, textures and sizes are used to those of the original roof covering.

4.26.2 Innovation or the use of new materials will be encouraged, except where it detracts from the character of an area.

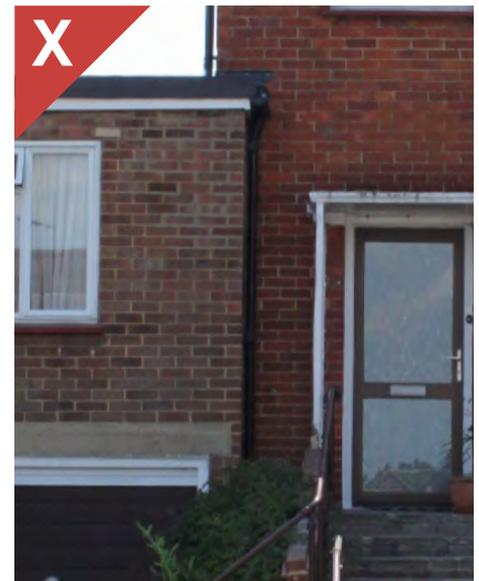


Figure 4.26b: An example of a side extension that attempts to match the existing brick work but fails to do so.



Figure 4.26c: An example of the successful introduction of contemporary materials in this metal clad side extension by HUT Architecture.



Figure 4.26d: An example of inappropriate cladding to a house that hinders the appearance of the street.

4.27 ARCHITECTURAL DETAILS & FEATURES

4.27.1 The architectural details and decorative features of a building significantly contribute to the appearance of a dwelling. Design proposals for extensions and alterations should consider the response to existing features and how new details may be introduced to add interest and respond to the local character.

- Where rebuilding part of the existing dwelling, the continuation of plinths, string courses, bond patterns, decorative brickwork, barge boards and fascias should be integrated into the design.
- Decorative features such as terracotta panels, carved bricks, glazed tiles, decorative ridge tiles and finials, lintels or plaques should be retained and restored, if damaged.
- Functional features, such as chimneys help provide rhythm to a street, particularly on semi-detached and terraced housing. Where chimneys are no longer used to service fireplaces, they can provide ventilation and reduce condensation within a home. Where they contribute to the original design, their retention is encouraged. Where a new chimney would be appropriate to the scale and position of an extension, they can assist with the integration into the suburban setting and provide a functional use.
- The addition of contemporary features and details will be encouraged where they respond to the design of the proposal, the existing house and the character of the local area.

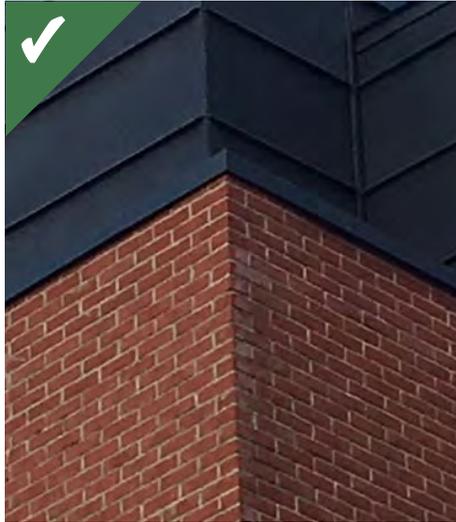


Figure 4.27a: Standing seam metal and brick.

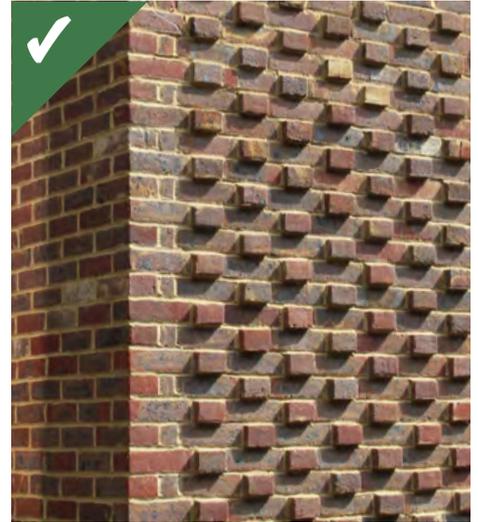


Figure 4.27d: Patterned brickwork.



Figure 4.27b: Crafted wooden shingles.



Figure 4.27e: Stepped courses of brickwork.

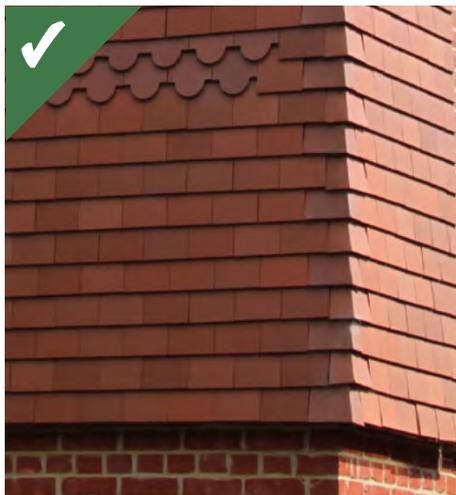


Figure 4.27c: Hung tiles, including decorative tiles.



Figure 4.27f: Slate tiles.

OUTBUILDINGS

4.28 BUILDING IN GARDENS

4.28.1 Outbuildings providing additional space associated with a dwelling, such as storage, a home office or summer house should not result in the creation of a separate dwelling and should share access, gardens and services with the main dwelling.

4.28.2 Such proposals may be required to demonstrate that the proposed outbuilding is ancillary to the existing house so as not to be considered a separate dwelling. Where an outbuilding would result in a separate dwelling, applicants should refer to the guidance on subdivision and rear garden development (Refer to Chapter 2).

4.28.3 Outbuildings should be designed to:

- Provide an ancillary function such as a home office, garage or storage.
- Be located in a position that provides access requirements relevant to the use, but should not be dominant in the street scene or in a location where they would appear to add clutter. Consideration should also be given to the level of natural surveillance over the outbuilding.
- Be of a scale that is subservient to the main house. The maximum height and footprint of an outbuilding should be determined on a case-by-case basis, dependent on the size of the plot, scale of the host building and impact on neighbouring amenity.
- Be innovative, standalone buildings. Outbuildings do not need to respond to the style of the host dwelling, except where visible from the street. Where visible from the street, proposals for outbuildings should respond to the character



Figure 4.28a: An example of an outbuilding providing an ancillary living space and home office designed by Surman Weston and Joseph Deane. (Photo: Wai Ming Ng)

of the existing dwelling.

- Ensure that where a garage is proposed, it should not directly abut a pavement or highway. The garage doors should not open onto a pavement or highway.
- Outbuildings and garages should generally be set behind the main building line.

4.28.4 If the outbuilding is to be used as a habitable space⁵⁴, proposals should consider heating (and insulation) and access to light and ventilation.

⁵⁴ Habitable spaces may include a home office or study.

FRONT GARDENS, PARKING & STORAGE

4.29 FRONT GARDEN DESIGN, INCLUDING PARKING

4.29.1 Except in certain circumstances⁵⁵, most front garden works do not require planning permission. All front garden works requiring planning permission should follow the guidance below. Where works do not require planning permission, homeowners should consider the following guidance to achieve the best possible outcome. Homeowners should also consider the need to notify neighbours under the Party Wall Act if proposed works may affect a shared boundary and generally for any proposed development.

4.29.2 The design of front gardens, including landscaping, can significantly enhance a home and the character of the street. Proposals for front gardens and forecourt parking should follow the guidance described in Figure 4.29a and:

- Provide parking which is proportionate to the size of the dwelling and avoid paving over a significant amount of the forecourt. Forecourts that are completely covered in hardstanding should be avoided; as a minimum a planted border along all boundaries should be provided.
- Allow sufficient space between the car and the dwelling to allow access to the front door and side of the property. Front garden parking must be designed to avoid cars

⁵⁵ Circumstances where planning permission is required include where the property is within a Conservation Area, where the works are dealing with a Tree Protection Order or where a proposal seeks to create a new driveway across the pavement. Applicants should contact Council's Planning Department before undertaking works.

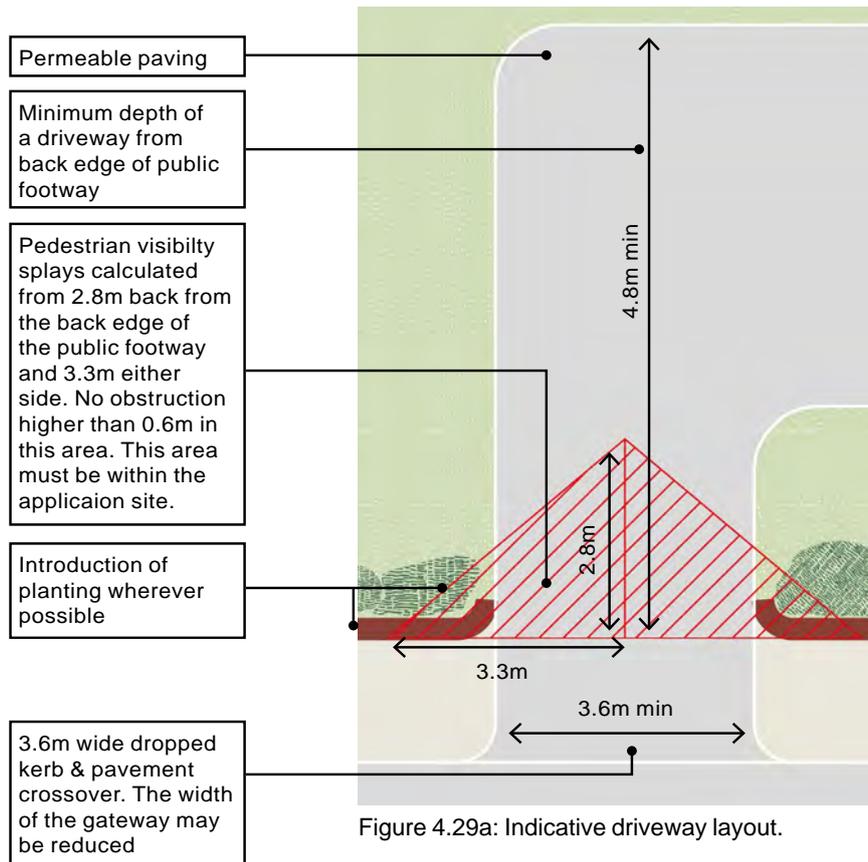


Figure 4.29a: Indicative driveway layout.

- overhanging the pavement.
- Should not include gates. Where gates already exist, they must not open outwards and should allow enough space for them to be opened inwardly (if relevant) whilst a car is parked in the forecourt. Gates should enable a pedestrian on the footway to have clear visibility of any vehicle exiting (i.e. they should be railings or have some form of transparency) and should not be of a height that blocks visibility of passing pedestrians and should enable visibility from the footway.
- Avoid the need to remove any existing trees or established hedges.
- Introduce new planting wherever possible.
- Introduce permeable paving to new areas of hardstanding to minimise rainwater run-off issues, as per the requirements of PD⁵⁶.

⁵⁶ For more information, refer to Schedule 2, Part 1, Class F, available at: http://www.legislation.gov.uk/ukxi/2015/596/pdfs/ukxi_20150596_en.pdf.

- There should be no water run-off from the forecourt onto the public highway.
- Repair or restore any original decorative tiled paths that are a characteristic or historic feature of the existing dwelling.
- Pedestrian and visibility splays for the crossover and vehicle access must be in accordance with Croydon Guidance⁵⁷.
- If a new dropped kerb and crossover is required then applicants must apply for and obtain consent via the Croydon Highways Department⁵⁸.

⁵⁷ Available at: <https://www.croydon.gov.uk/transportandstreets/rhps>

⁵⁸ For advice, refer to: <https://www.croydon.gov.uk/sites/default/files/articles/downloads/VCO%20application%20Sep%202016.pdf> and <https://www.croydon.gov.uk/sites/default/files/articles/downloads/Residential%20Driveways%20and%20Car%20Accesses.pdf>.

4.30 FRONT GARDEN BOUNDARY TREATMENTS

4.30.1 Boundary treatments help to define the relationship between a dwelling and the street. They can include garden walls, fences, railings and hedges. A strong front boundary treatment should be incorporated into proposals, particularly where this is characteristic of the street. Boundary treatments visible from the street should:

- Respond to the design of the dwelling;
- Be consistent with the height of other enclosures on the road;
- Avoid the introduction of different styles along the street. Treatments should reinforce the dominant boundary type along the street, ensuring consistency with the style and age of the property;
- Consider well-maintained planting as an alternative solution and retain any hedgerow;
- Incorporate visibility splays and sight lines for pedestrian and vehicular safety.

4.31 REFUSE & CYCLE STORAGE

4.31.1 Refuse and bicycles often create clutter on the street scene. Dedicated external storage can resolve the impact on the character of an area. Where possible, this should be located in a discreet location to the side or rear of a property.

4.31.2 Where storage is located in front of a property, it should be:

- Located away from the front boundary and in a discreet location where it does not intrude on the street scene;

and

- Be of a design that does not negatively impact the setting of the dwelling or local character. Simple wooden structures or simple metal storage products (Refer to Figure 4.31a and 4.31b) surrounded by landscaping are a common and effective solution, where structurally secure and with a Police security recommendation.



Figure 4.31a: An example of a secure cycle store located in an easily accessible position. (Photo: Trimetals Ltd.)



Figure 4.31b: Ancillary storage for individual homes are best accommodated in the front garden of a property where they are well integrated into landscaping, as demonstrated in the bin stores alongside. (Photo: www.bikebox.london/)

An isometric illustration of a city street scene. The scene is viewed from an elevated perspective, showing a grid of streets. Buildings are rendered in a stylized, blocky manner with various colors like red, orange, and grey. There are several trees with green and yellow foliage scattered throughout the scene. A few vehicles, including a red car, a white van, and a motorcycle, are visible on the streets. The overall style is clean and modern, with a focus on geometric shapes and a warm color palette.

CHAPTER 5: APPENDIX

GLOSSARY

ARCHAEOLOGICAL PRIORITY AREAS:

Areas that are known to be of archaeological importance because of past finds, excavations or historical evidence.

BACK LAND:

Land behind an area which can be built on or otherwise developed. In some instances, this may include land that is already developed.

CONSERVATION AREAS:

Conservation Areas are designated by the Local Planning Authority. A Conservation Area is an area of special historic or architectural interest that make it unique and are protected by law against undesirable changes. The main attributes that usually define the special character of a Conservation Area are its history and physical appearance, including building form, materials and architectural style. The Planning (Listed Buildings and Conservation Areas) Act 1990 defines the quality of a Conservation Area as being: "the character or appearance of which it is desirable to preserve or enhance". Croydon Council provides specific guidance for these areas in the Conservation Area General Guidance and Conservation Area Appraisal and Management Plans.

CROYDON LOCAL PLAN:

The planning policy document that sets out the spatial vision and plan for the future of the borough and how it will be delivered.

EVOLUTION WITHOUT SIGNIFICANT CHANGE OF AN AREA'S CHARACTER:

This is detailed in the Croydon Local Plan Policies DM10.1-10.10 and is relevant to Chapter 2 of this SPD2.

FLEXIBLE BUS:

This refers to demand-responsive transport where vehicles alter their routes based on particular transport demand rather than using a fixed route or timetable and can be used to provide a public transport service in areas of lower densities where a regular bus service is not considered to be financially viable.

FOCUSSED INTENSIFICATION ASSOCIATED WITH CHANGE OF AREA'S LOCAL CHARACTER:

This is detailed in the Croydon Local Plan Policies DM10.11 and is relevant to Chapter 3 of this SPD.

FORECOURT:

An area to the front of a building which may typically include landscaping and some space for parking.

GUIDED INTENSIFICATION:

This is detailed in the Croydon Local Plan Policies DM34-39 which provide place specific policies for development.

HABITABLE ROOMS:

Habitable rooms are described as any room designed *and* used for sleeping, cooking, living or eating purposes. Undersized rooms performing these functions will not be considered habitable rooms, and this will be judged on a case by case basis. Enclosed spaces such as bathrooms or toilet facilities, service rooms, corridors, laundries, hallways, utility rooms or similar spaces are not considered to be habitable rooms. Ancillary spaces in a separate structure to the main dwelling, such as garden rooms, are not considered to be habitable rooms.

HARDSTANDING:

An area of paved ground (for example with tiles, bricks, pavers or concrete etc.) that sits outside the external envelope of a property. This may include driveways and patios.

HERITAGE ASSET:

A building, monument, site, place, area or landscape identified as having a degree of significance meriting consideration in planning decisions, because of its heritage interest. Heritage asset includes designated heritage assets and assets identified by the local planning authority (including local listing).

HIGH QUALITY DESIGN:

A design that has a developed approach to character, with a massing and coherent form that responds to the site context and provides good standards of accommodation and amenity, internally and externally, making use of quality materials and detailing.

HIT & MISS BRICKWORK OR STONE:

Brick or stone that is stacked with spaces between the bricks/stones. These spaces are generally of a size that reduces perceived overlooking sufficiently whilst allowing light to filter through.

HOST DWELLING:

This is a dwelling which is under the same ownership and forms part of the original plot on which development is proposed.

INCOHERENT FORM:

The shape (in plan and/or section and/or elevation) of a development that is overly complicated and negatively impacts the appearance of a proposal.

LISTED BUILDINGS:

Means a building which is for the time being included in a list compiled or approved by the Secretary of State under this section; and for the purposes of this Act -

(a) any object or structure fixed to the building;

(b) any object or structure within the curtilage of the building which, although not fixed to the building, forms part of the land and has done so since before 1st July 1948, shall subject to subsection (5A)(a) be treated as part of the building.

LOCAL HERITAGE AREAS:

Local Heritage Areas (LHA) is a designation based upon the original criteria for designating Local Areas of Special Character. The criteria for designating an area as an LHA were consulted upon in autumn 2013 as part of the Croydon Local Plan: Detailed Policies (Preferred and Alternative Options). To be designated as an LHA an area:

- Must be of heritage significance; and

- Meet one or more of the following three criteria:

1. Architecture – the architecture in the area must be of a high quality, distinctive and well preserved. This is because the collective value of a group of historic buildings has a consistent architectural form, style, features, detailing or materials and the group will often, but not always, have been built as a single development over a short period of time.
2. Townscape – the townscape of the area must be of a high quality, distinctive and well preserved. This is because of the attractive and historic composition of the urban form and the area will often, but not always, have been planned.
3. Landscape – the landscape of the area must be of a high quality, distinctive and well preserved. This is because of the distinguishing quality, extent or features of its historic landscape and it will often, but not always, have been planned.

LOCALLY LISTED BUILDINGS:

The locally listed buildings and structures within Croydon are considered by the public and the Council as having special local architectural or historic interest, to be of significance to the local community and to contribute to the environmental and cultural heritage of the borough. All locally listed buildings should satisfy at least two of the following criteria:

- **Authenticity:** Buildings and groups selected for the local list should be substantially unaltered and retain the majority of their original features;
- **Architectural significance:** Buildings which are of good architectural quality or are good examples of a particular building type;
- **Historical significance:** Buildings which represent specific architectural and social building periods or which are associated with, local historical events, the development of Croydon, well known people or noted designers;
- **Technical significance:** Buildings that display exceptional innovation and craftsmanship;
- **Townscape Value:** Buildings and/or groups of buildings which due to their form, massing, appearance and positive role in the streetscene, such as key corner buildings, local landmarks, uniformly designed terraces.

LOCALLY LISTED HISTORIC PARKS AND GARDENS:

The criteria for the local list of historic parks and gardens are set by Historic England along with the London Parks and Gardens Trust. This stipulates that the site have at least one of the following:

- evident historic features or design
- buried archaeology
- connections with famous designers or nurserymen
- connections with nationally or locally famous individuals
- a design typical of a landscape style.

LONDON PLAN:

The strategic plan for London, setting out an economic, environmental, transport and social framework for development.

NATIONAL PLANNING POLICY FRAMEWORK (NPPF):

The National Planning Policy Framework is a key part of Government reforms to make the planning system less complex and more accessible, and to promote sustainable growth. The NPPF replaces all existing Planning Policy Statements and Planning Policy Guidance.

OUTRIGGER:

An addition to the main building mass, commonly through a rear extension that is not full-width. These are common in terraced houses and often are part of the original dwelling.

OVERBEARING:

Where a development is considered to have an unreasonable impact on neighbouring and local amenity as a result of its scale and appearance. The level of reasonableness is dependent on the site context and must be balanced by the need to provide homes. Developments that adhere to the guidance within SPD2 will generally not be considered to be overbearing, however this will be judged on a case by case basis.

PERMITTED DEVELOPMENT:

Permitted development rights are a national grant of planning permission which allow certain building works and changes of use to be carried out without having to make a planning application.

RAINWATER GOODS:

Items attached to a dwelling to remove rainwater from roofs into the drainage system.

REAR GARDEN DEVELOPMENT:

The construction of new buildings in rear gardens of the existing properties.

REGISTERED PARKS AND GARDENS:

A site included on the Register of Historic Parks and Gardens in England. Registered parks and gardens are designated heritage assets and subject to the planning policies within the NPPF.

SCHEDULED MONUMENTS:

Means any monument which is for the time being included in the schedule (compiled and maintained by the Secretary of State for Culture, Media and Sport).

SELF-PROVIDED HOUSING:

This is the delivery of housing that is steered by the future resident(s). This commonly includes community-led, self-build and custom build housing.

SETTING OF A HERITAGE ASSET:

The surroundings in which a heritage asset is experienced. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral.

SIGNIFICANCE:

- 1) The value of a heritage asset to this and future generations because of its heritage interest. That interest may be archaeological, architectural, artistic or historic. Significance derives not only from a heritage asset's physical presence, but also from its setting.
- 2) The sum of the cultural and natural heritage values of a place, often set out in a statement of significance.

STREET:

A road that is either adopted or to an adoptable standard, and does not include rear access lanes or driveways.

SUBURB:

A predominantly residential area that has emerged on the outskirts of district and metropolitan centres.

TRANSPORT FOR LONDON (TFL):

Transport for London (TfL) is the organisation responsible for London's transport system. Its role is to implement the Mayor's Transport Strategy and manage transport services in London, for which the Mayor has ultimate responsibility.

UN-NEIGHBOURLY WINDOWS:

Where such a window already exists on any type of development (including windows within extensions), it is a window that faces onto a potential development site in a way that would not be permitted if the window did not currently exist as it would unreasonably preclude development on the neighbouring site. Where such a window does not currently exist, it is a window proposed within any type of development application (including extensions) that would unduly preclude development on the neighbouring site.

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